

Br.P 95-92

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of the East*

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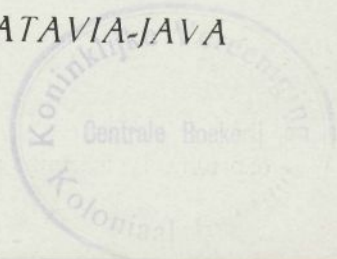
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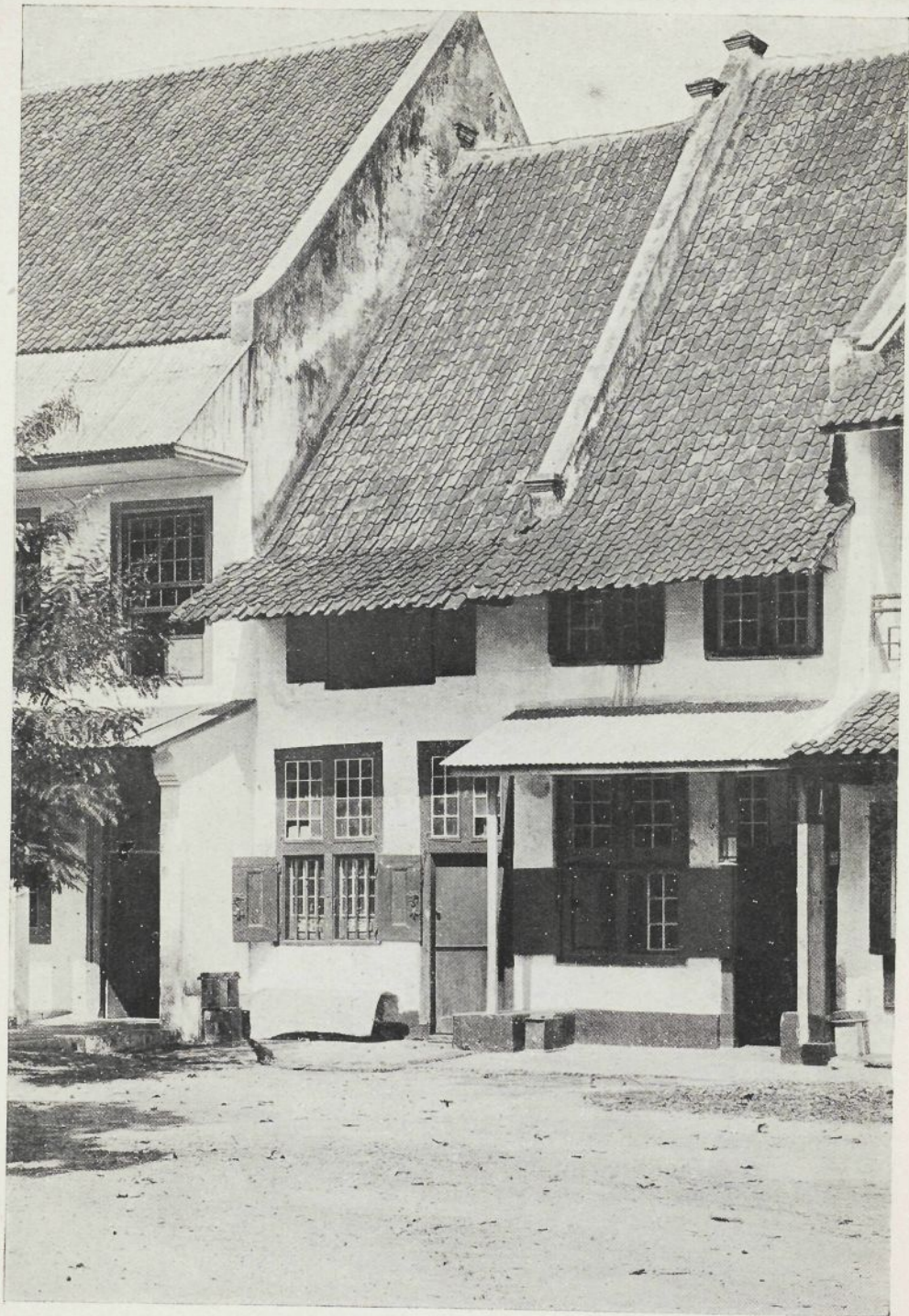
# BATAVIA

QUEEN CITY  
OF THE EAST



Compiled by G. G. VAN DER KOP  
Published by G. KOLFF & Co. - BATAVIA-JAVA





OLD BATAVIA: PICTURESQUE DUTCH HOUSES DATING FROM THE 17th CENTURY.

## THE STORY OF BATAVIA

*The founding of the capital of  
the Netherlands East Indies.*

### *Earliest European visitors to Java.*

Spices, precious stones and other valuable merchandise came in olden times from the East, and were transported by sea and caravan to the markets in Europe.

To obtain those goods first hand, and thereby saving the enormous cost of transport and many transit ports not to mention the profits of middle-men, the Portuguese at the end of the 15th century began to search for the lands where they originated.

Vasco da Gama sailed in 1497 and reached the present Calcutta by way of the Cape of Good Hope. In 1499 a second expedition sailed, which eventually also reached Calcutta after first touching at Brazil. Several expeditions followed and trade with India was firmly established, which in time led to the visiting of more remote countries.

It was as far back as A.D. 1511 that the Portuguese first came to Java when Antonio de Abreu, one of the lieutenants of the well known d'Albuquerque called at Tuban and Grisee, on the same voyage also visiting Amboina and Banda. The next voyage took place in 1522, when a certain de Lerne was sent to Bantam, which at that time was still a Hindu empire.

The ruler of Bantam was at war with the Mohammedan prince of Cheribon and Mohammedan missionaries in Bantam used all their influence to convert the Hindu inhabitants to the new religion.

The King of Bantam fore-seeing the ultimate downfall of his empire, was inclined to make terms which the Portuguese. He promised them a site for the founding of a factory, freedom of trade and an annual gift of a thousand bags of pepper, if only the Portuguese would build a fortress to defend the port and enter into an alliance with him against Cheribon. The Portuguese agreed, but as their force at that time was not strong enough, they sailed away promising to return with reinforcements. However they stayed away longer than they had intended and, when coming back in 1517 they found that the Sultan of Cheribon had conquered Bantam and established his rule.

These expeditions led to the founding of the trade of the Portuguese with the East Indies and during the years following this trade came wholly in their hands, the period of its highest development being from 1590 till 1610. During that time a single fleet of their ships would sometimes number from 150 to 250 sail.

Towards the end of the 16th century the Indian trade had assumed immense proportions and Lisbon became the richest port of Europe.

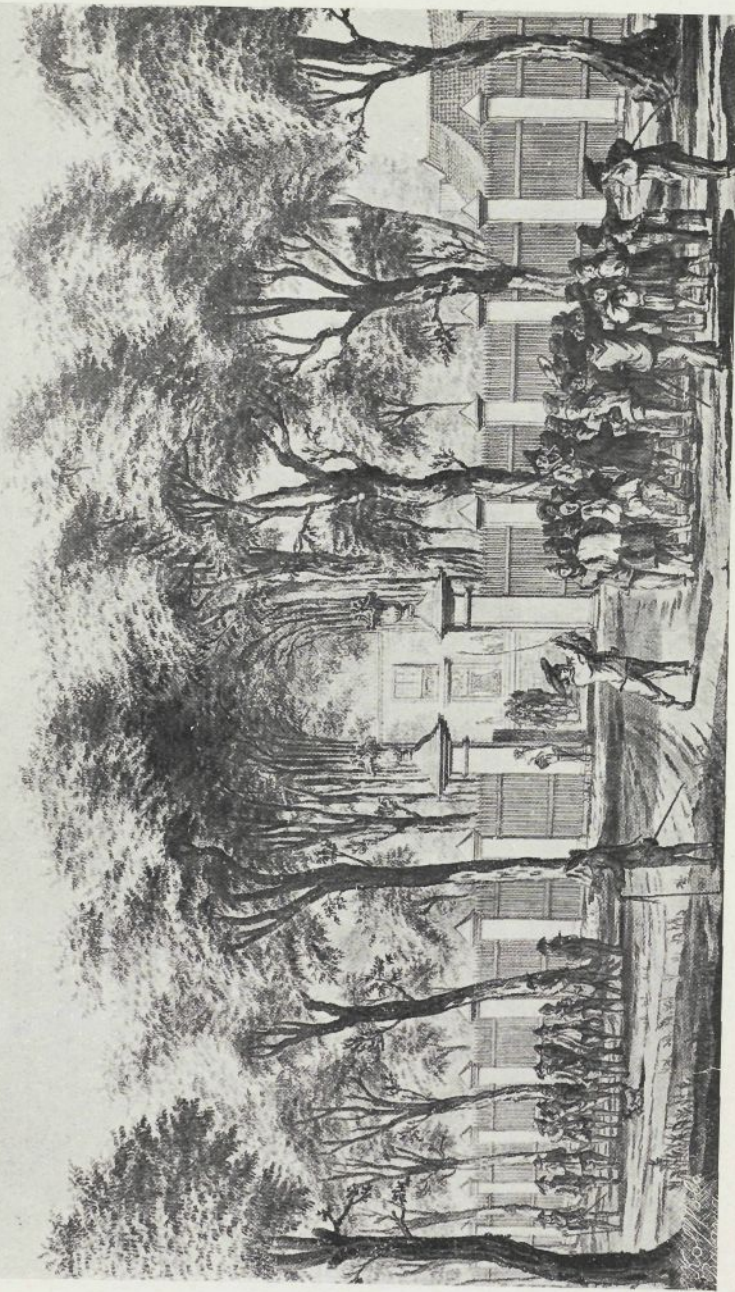
Whilst Lisbon was the headquarters of this trade, the Dutch ports had become the chief distributing centres which caused the envy of other maritime nations.

In 1577 when Drake made his memorable voyage round the world, the English called at the Moluccas and Bantam and brought back to England a cargo of spices and pepper from the East besides a full report of the possibilities and advantages of the trade with this part of the world, which caused the public in England to fully realise that the opening of such a trade would be of great importance.

Nevertheless the second visit of the English did not take place before 1587 when Thomas Cavendish or Candish called at Bantam with three ships and, after taking in a full cargo, returned to England in 1588.

These two visits were followed by a third in 1591, three ships being sent, however only one reached its destination, one being sent back to England with sick men, and the third one foundering.

*HET GEZICHT VAN HET BUITEN HOSPITAAL VAN VOOREN AAN DE  
 ZIEN BY DE POORT NOORDWYK, 37 JAAREN BUITEN BATAVIA.*



OLD BATAVIA: REPRODUCTION FROM AN OLD PRINT. SCENE IN 18TH CENTURY BATAVIA IN FRONT  
 OF THE HOSPITAL, WHICH HAS LONG SINCE DISAPPEARED.

The matter than lapsed in England, only to be revived when rumours came about the first expedition of the Dutch to the East and their successful return. On December 1600 a charter was granted by Queen Elisabeth to some merchants of London, for the creation of a Company trading to the East Indies. As soon as the charter was granted the Company began to raise a joint stock for carrying the project into execution and in a very short time £ 72.000 was raised which enabled the new company to equip five ships to trade with the East Indies. This fleet sailed in May 1601. Lancaster, the head of the expedition managed to enter into a treaty with the Prince of Acheen on behalf of the East India Company, which treaty was the first one to be concluded between a native Prince and this Company. From Acheen Lancaster proceeded to Bantam. A treaty was concluded and an English factory opened, where the goods from the ships and the cargo for England was stored. His ships then loaded for their return with the exception of two which had already sailed for home with a full cargo from Acheen. This was the beginning of the trade of the English with the East Indies.

The Dutch in the mean time had been patiently awaited their time. In 1594 the closing of the port of Lisbon to the ships of the Dutch put an end to their profitable trade and everything was done to collect all possible information about the route to the East Indies, in order to be able to establish a direct trade with the East Indies.

This was made possible by the return from Portugal to his own country of the Houtman at the risk of his life, who had been trying to obtain all possible information about the so jealously guarded route to the East. He succeeded in procuring sea charts and merchants in Amsterdam formed a company, called the Company for Remote Lands. At the first meeting of the directors of this company it was resolved to equip four vessels to be despatched to the East Indies, under the command of Cornelis de Houtman.



OLD BATAVIA: ONE OF THE SIX CHURCHES IN 18th CENTURY BATAVIA.



OLD BATAVIA: THE PORTUGUESE CHURCH. NEAR THE ENTRANCE IS THE TOMBSTONE  
OF THE GOVERNOR-GENERAL ZWAERDECROON.

This small fleet consisting of the "Maurits" and "Hollandia" of 400 tons each, the "Amsterdam" of 200 tons and the "Duyfje" of 50 tons, sailed from Holland the 2nd of April 1595 and after an eventful voyage, pursued by gales and hurricanes, the crews of the ships decimated by sickness, they sighted Sumatra on January 1st 1596 and reached the Straits of Sunda in February, arriving at Bantam on June 23rd.

Here they found the Portuguese firmly established and, as it was soon known that they only came to trade, they met with a friendly reception. A treaty was concluded between the Sultan of Bantam and de Houtman which gave the latter full liberty to buy whatever he wanted.

Later on the Portuguese became jealous of the favours which the Dutch were receiving and started intriguing against them. As a result in September de Houtman and nine of his shipmates, were arrested by the Sultan. After a good deal of parleying the prisoners were ransomed on the condition that they would immediately leave the shore of Bantam. The fleet then sailed for Jacatra, the capital of a small state between the rivers of Tangerang and Tjitarum, which was tributary to Bantam, and thence for Tuban and Sidaju, from where they continued their voyage to Madura. Next they visited Bali which was the last place of call during their voyage and the fleet sailed homeward with the exception of the "Amsterdam", which on account of leakage had to be abandoned. The "Maurits" and "Hollandia" reached Texel in August 1597.

Great was the sensation created in mercantile circles; the shrewd merchants saw at once that the whole trade with the East Indies was at their disposal and need not any longer be monopolised by the Portuguese.

Another expedition was organized and in 1598 six ships and two yachts under the command of Jan Cornelis van Neck sailed for the East. The whole fleet reached Bantam in due course and a profitable trade was done. Notwithstanding the efforts of the Portuguese no trouble occurred as the native princes began to find that they had to deal with real merchants, who paid liberally for what they received and only desired to trade.

Four of the ships returned homeward with a full cargo, the remainder of the fleet going on to Amboina where they were cordially welcomed, the inhabitants looking upon them as the ones who would relieve them from the cruelty and tyranny of the Portuguese. They returned in 1599 to Holland after first having called again at Bantam.

The Dutch were now on the road to success and determined to sweep the Portuguese off the Eastern seas. When the latter realised this they sent a fleet of 30 men of war to cut off Dutch ships sailing to the East. They attacked a Dutch squadron of 8 ships but were badly beaten, some Portuguese ships which were richly laden being taken and brought to Holland. After this the trade of the Portuguese dwindled away and it was not long before the Portuguese were driven altogether out of the East.

Elated with the success of the trade many companies were formed in Holland with the purpose of trading with the East. This created trouble as now Dutch were competing with Dutch, thus lowering the prices of their own goods whilst the prices of goods to be bought were raised.

It was soon observed by the States General of the United Netherlands that this condition of affairs would lead to a total ruin of the East Indian trade, and in 1602 a meeting was called of the Directors of the several existing companies in order to come to an understanding. From this meeting the founding of "De Vereenigde Oost Indische Compagnie" (United East India Company) resulted of which the initials V. O. C. were to become so well known throughout the world.

The Company received a patent from the sovereign power for 21 years, beginning from March 1602; its joint-stock capital was 6,600,000 guilders (£550,000) and the power granted to it for conducting the affairs in the East was practically unlimited.



OLD BATAVIA: INTERIOR OF THE PORTUGUESE CHURCH.

The first expedition to be sent by this company comprised a fleet of 14 large ships which sailed from Holland in June 1602. After this fleet after fleet was rushed to the East and if to-day the Dutch are masters in the East-Indian Archipelago, they have only themselves to thank for it; it was again that, with their proverbial slowness, once they have made up their minds that something can be done they do it.

The English in the mean time, noticing the success of the Dutch, awakened and the fact was brought home to them that, if ever they wanted to have a part in this rich trade, they ought to move and do something. This led to the second expedition under Lancaster. The rivalry of the two great nations of that time led to continued quarrels over petty trading matters. The English maintained that they had a greater right to the spice markets than the Dutch in as much as they had been there before them, to which the Dutch retorted that their rights were first, as Cornelis de Houtman came here in 1596 with a charter of his lawful sovereign in his pocket and in their name not only planted the Dutch flag in Bantam, but founded factories in the Molucca's, this being quite another thing to the action of half-pirates who had sailed around the world and accidentally called at the East Indies, further pointing out that the first Englishman who came here with a charter of his sovereign — Lancaster — did not reach the East Indies before 1602 — 1603, at which time the Dutch were already established there.

Here were the elements for strife which in later years would be more and more accentuated. It was not to be expected that quarrels would be settled amiably and it can be well understood that every opportunity was taken by individual members of the two communities to make things as unpleasant for the other as they possibly could.

In 1610 the Dutch administrative organization in the East was shaped into a more concrete form by the appointment of a Governor-General. The first man to fill this position was Pieter Both, who had been an admiral of one of the fleets to the East. He was appointed by resolution of the States General of the United Netherlands of November 29th 1609 and left Holland in January 1610 to arrive in Bantam in December of the same year.

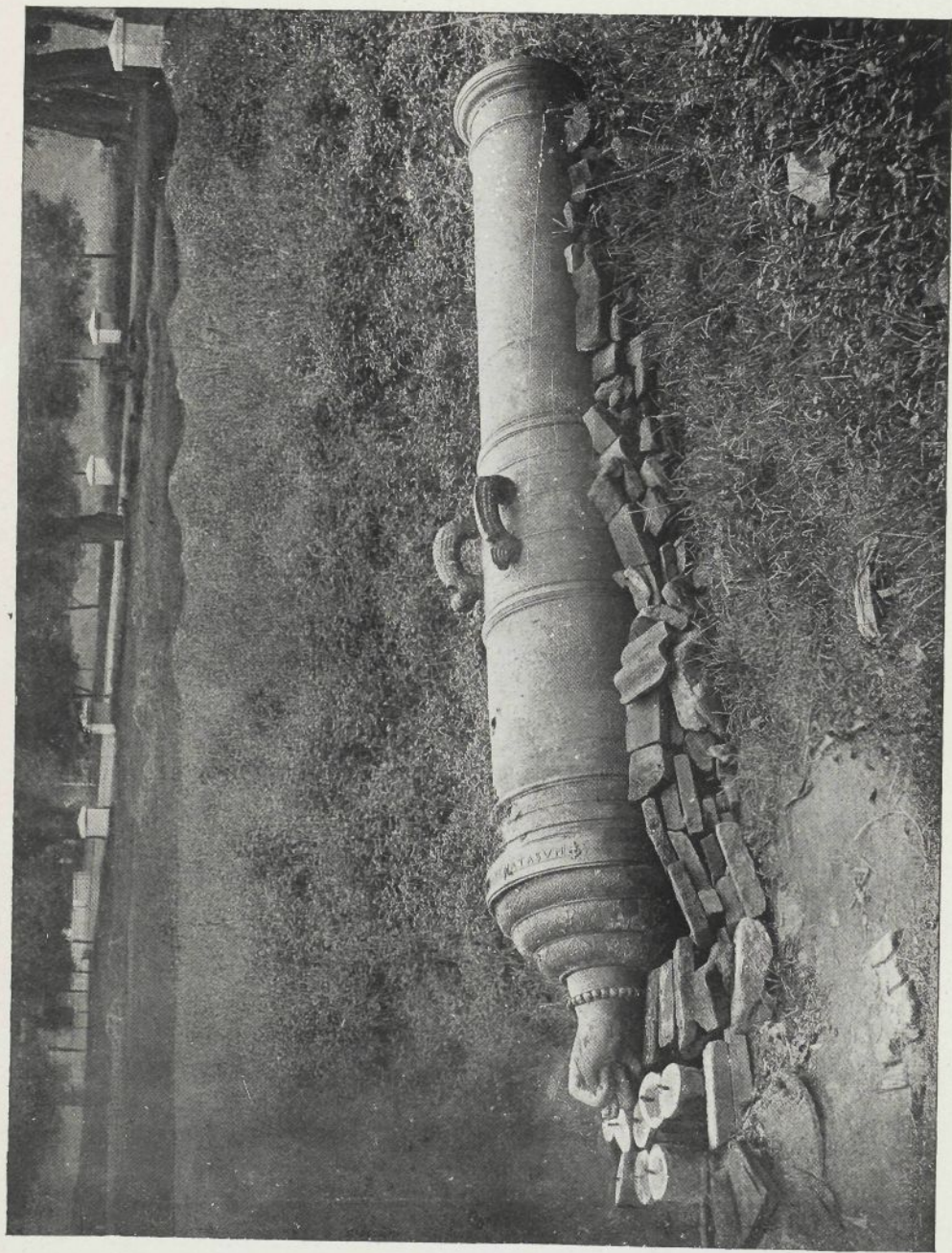
He was in office till November 1614 and was succeeded by Gerard Reynst, one of the members of the Committee of the V. O. C. in Holland. When returning to Holland in the ship Delft, Both was shipwrecked and drowned off the island Mauritius.

General Reynst only reigned just one year, dying the 20th December 1615 at Jacatra. At a meeting of the members of the council in East India, Laurens Reael was nominated as his successor. At the time of his appointment Reael held the office of Governor of the Moluccas.

For many years previously there had been trouble between the authorities in Bantam and the Dutch, arising out of the claim of the latter to monopolise the trade on the strength of agreements concluded with the rulers of Bantam. It was during the reign of Pieter Both in 1611, that the disputes reached such a point that a withdrawal from Bantam was proposed.

This the Dutch did, they moved to Jacatra and founded a factory there. The change from Bantam to Jacatra was principally due to political considerations but Bantam had also many disadvantages as is proved by the records of both English and Dutch, one of these being its unhealthy climate.

It was on this last pretext that the English, after that the settlement of the Dutch in Jacatra had been built, also established a factory at that place, at the same time keeping an establishment in Bantam. Relations in the mean time became strained and the rulers of Bantam and Jacatra entered into an alliance against the Dutch, with the English, more or less as a sleeping partner, in the plot. The outlook finally became so threatening that the Dutch decided to fortify their factory, at the same time constructing a redoubt on the island of Onrust, one of the



OLD BATAVIA: THE SACRED CANNON NEAR THE PENANG GATE.

smaller islands on the roadstead of Jacatra, which during a certain length of time had already been the place where the Dutch ships refitted.

Reael in the mean time had written to Holland to be relieved of his office, and in October 1617, the Board of Directors in Holland resolved to nominate Jan Pietersz. Coen as his successor. Coen, who was to become one of the most famous of the Governor-Generals who have reigned in the East

Indies, was born in 1587 in Hoorn, a small town in Holland on the Zuydersea. For the first time he went to East India in 1607, returning from there in 1611, and leaving for the East again in 1612; after arrival he soon rose in rank. First he was appointed president of the factory in Bantam and afterwards, when this factory moved to Jacatra, raised to the rank of Director-General. As his nomination to the office of Governor-General only arrived in Java in 1618, it was not before that time that he succeeded Reael.

It was Coen who decided that Jacatra ought to be strengthened and in fact it was also Coen who at that time already held the reins. It was he who wrote in 1617 to the Directors at home when the question of a principal settlement and meeting place, instead of Amboina which was only nominally so, was being discussed! "General Reael has asked my advice as to which place in my opinion must be given preference as residence of the Governor-General and general meeting and transit place. I have answered this letter amply and stated that according to my views it ought to be somewhere round here at the same time advising him (Reael) to come here. I hope and think that when H. E. arrives, it will be of great advantage to the Hon. Company, as now we are not in agreement as to which means to use, and the cardinal points on which the welfare of the Company is depending in the opinion of some must be reached by force, whilst others think that a better result may be obtained when using milder means. A conference may lead to an agreement as to which of the two ways may be considered best".

Contrary to Reael, Coen was a man of deeds and, although he knew that a storm was brewing in the Moluccas he strengthened the Dutch settlement in Jacatra in spite of the protest of the ruler of that state, and when the English tried to accentuate this dispute he wrote the famous sentence, the first words of which are often quoted! "Do not despair, do not spare your enemies, there is nothing in the world which can thwart or try us for the Lord is with us! and do not look back upon faults made, for there is something great to be accomplished in India".

Affairs in the mean time had reached a precarious state. The English in Bantam, after having been reinforced by a fleet of five ships under Thomas Dale and William Parker and now having a fleet of fifteen sail at their disposal, open-



OLD BATAVIA, ORIENTAL VENICE.

ed the hostilities by the taking of the Dutch ship "Zwarte

Leeuw" (Black Lion) and to a protest from Coen no answer was vouchsafed but an insolent verbal one.

It was then that Coen decided not to wait any longer but on October 22nd attacked the English factory at Jacatra, taking the guns and burning down the whole English settlement. Another attack, some days later, made with the intention of destroying a battery built at the mouth of the river and which intercepted intercourse between our vessels and our factory, did not meet with success.

On December

29th rumours came to Coen that the English fleet had sailed from Bantam and on January 1st 1616, this fleet was sighted off Jacatra. There was no time for consideration and Coen, after entrusting Pieter van den Broeck with the command of Jacatra, on the 2nd of January sailed to meet the English fleet. A battle which was neither very murderous nor decisive was fought, but when the next day the English were reinforced by some more ships, Coen, who foresaw that in a battle his fleet would be defeated by the so much stronger English fleet, sailed for Amboina for reinforcements, leaving orders that Jacatra must be held at any cost.

Though the English had succeeded in driving off the Dutch fleet, and by doing so had placed the garrison of the fort in a perilous position, they were not decisive in their further actions. The obvious plan was to pursue the enemy



OLD BATAVIA: STAIRCASE IN THE RESTORED BANK OF INDIA BUILDING.



OLD BATAVIA; GENERAL VIEW OF THE KALI BESAR, IN THE BACKGROUND MODERN OFFICE BUILDINGS,

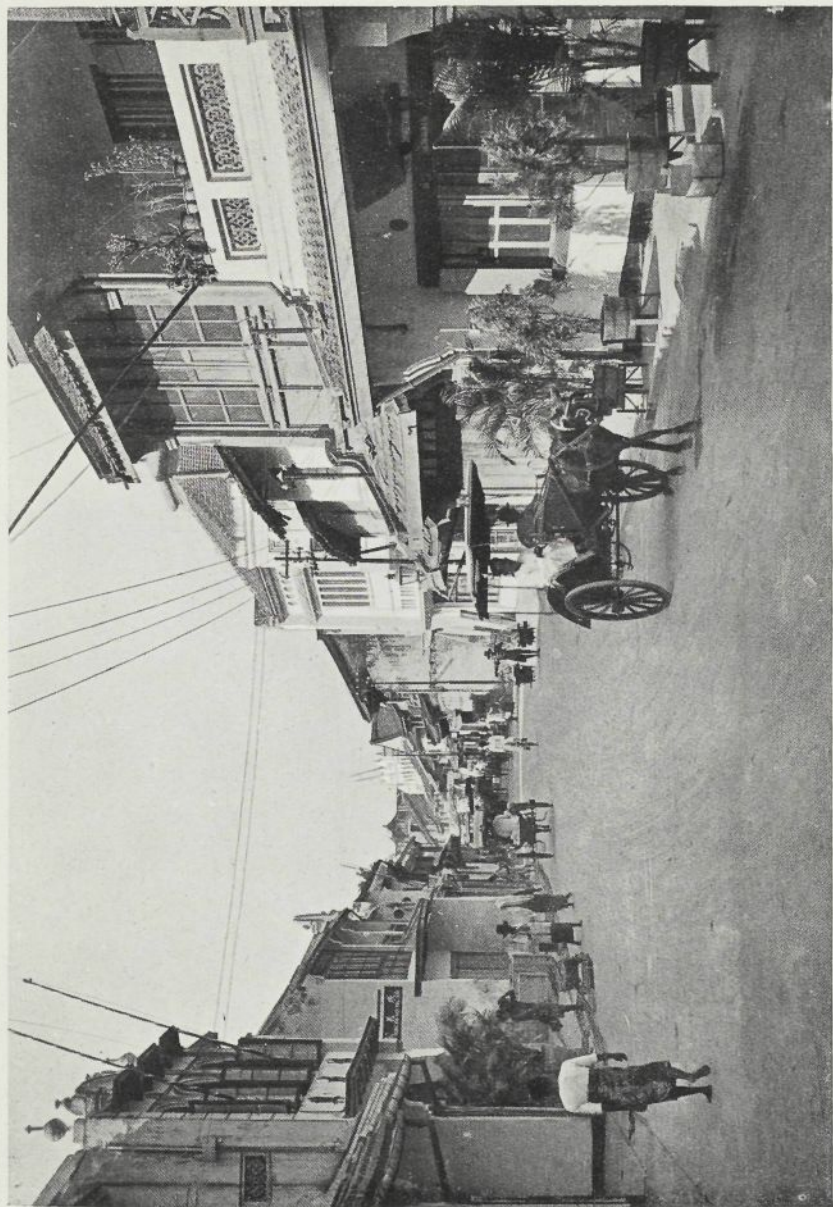
and to annihilate or disperse him. But there were several reasons against the adoption of this course. In the first place politically it was advisable that the close relations already formed with the rulers of Jacatra and Bantam should be maintained and strengthened and this could only be done by the fleet remaining in Javanese waters. Another consideration



NEAR "PASAR IKAN" IN OLD BATAVIA.

which restrained them from pursuing an active policy was the lack of provisions in the English settlements. Further news came that the Dutch were concentrating a strong force in the Molucca's and as it was known that the Dutch had a considerable force there, it was decided that it would not be advisable to risk what was practically the whole strength of the English, in an action, more especially as there were well grounded reports that a fresh Dutch squadron was on its way to the East Indies. Consequently it was decided not to proceed to the Molucca's but to aid the King of Jacatra in the hope of being able to supplant the Dutch.

After Coen's departure the work of strengthening Jacatra was resumed night and day and on January 13th, all the four redoubts of the fortress were completed. It was then that the Dutch were caught napping. The ruler of Jacatra, understanding that there would be little hope of ever taking the place by force, seemingly wanted to come to terms and when van den Broeck agreed to this, he was invited to come to the residence of the Pangeran (Prince) of Jacatra, in order to celebrate the compromise. When going there he, with seven others, was taken prisoner and negotiations opened with the result that van den Broeck's successor, Pieter van Raay, capitulated on the 31st of January, the terms being that the fortress should be surrendered to the English and Jacatrans, van den Broeck and his comrades liberated, and he together with the whole garrison be brought in safety on board the English fleet. Measures to evacuate the fortress were already taken, when Bantam stepped in. The Sultan of that state who wanted neither the English nor the Dutch in Jacatra, ordered the Pangeran of Jacatra to deliver the Dutch prisoners to him as this was really an affair of Bantam. As a result the Pangeran of Jacatra was forced to take flight and the state of Jacatra was made part of Bantam. He then demanded that the fortress should be given up to him and for the second time it was decided to surrender. However when the Dutch noticed that the English and the Sultan of Bantam could not agree as to the terms and that no attack was being made, they decided that it would be of greater advantage to try to hold the fortress and on the 12th of March, van Raay, collecting the whole garrison, christened the fortress Batavia under the tolling



ONE OF THE MAIN STREETS IN THE CHINESE QUARTER OF OLD BATAVIA.

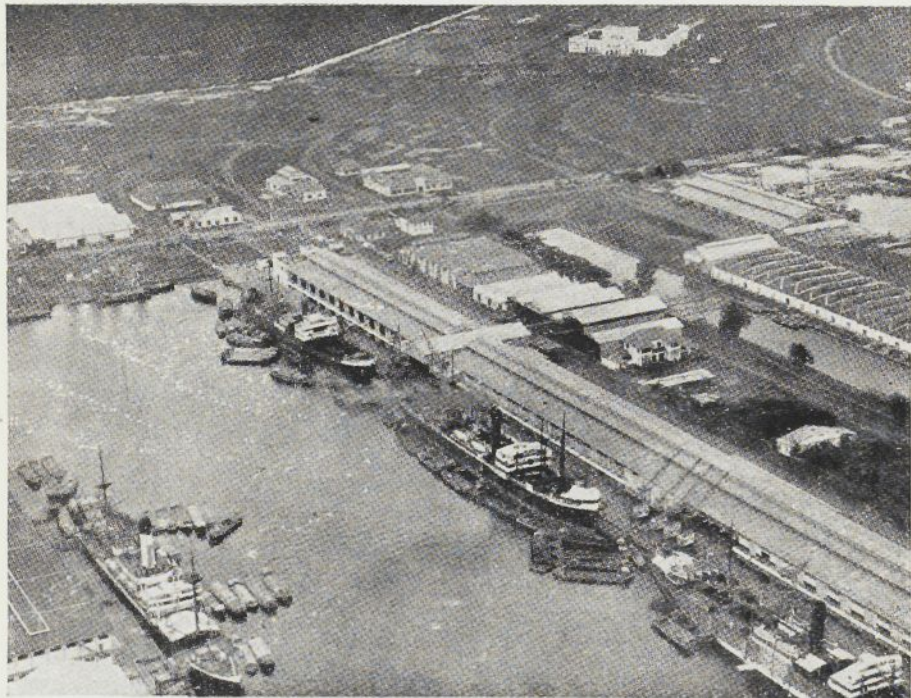
of the bells, and the hoisting of flags. To the four redoubts the names were given of Holland, West-Frisia, Zealand and Gelderland.

For two months the flag remained flying only to be taken in when rumours came of an intended attack by the natives.

On May 10th a sloop was sighted rowing up the river in which two members of the Council of India were seated, who brought news of the coming of Coen, and indeed some days later Coen himself arrived with a fleet of 16 sail. On May 28th Coen saw again the place he had founded and immediately after his arrival held a meeting with the Council in which was resolved that Batavia henceforth should be the capital of the possessions and the general meeting place for the fleets from and to Holland.

Events now marched rapidly, as both the English and the Sultan of Bantam had with-drawn their ships and forces when they beheld the display of naval strength by the Dutch. On May 30th Coen, destroyed the redoubts of the enemy without meeting with much resistance, nevertheless the whole native settlement was burnt.

Such was the founding of Batavia which laid the foundation stone for Dutch supremacy in the East Indies. From this small beginning rose the mighty city which afterwards would not inaptly be called the "Queen of the East" and which in latter years would play such an important role in commercial and political events in the Far East.



TANDJONG PRIOK: AERIAL VIEW OF THE 2nd INNER HARBOUR BASIN.  
IN THE BACKGROUND THE NEW RAILWAY STATION.

## HISTORIC BATAVIA.

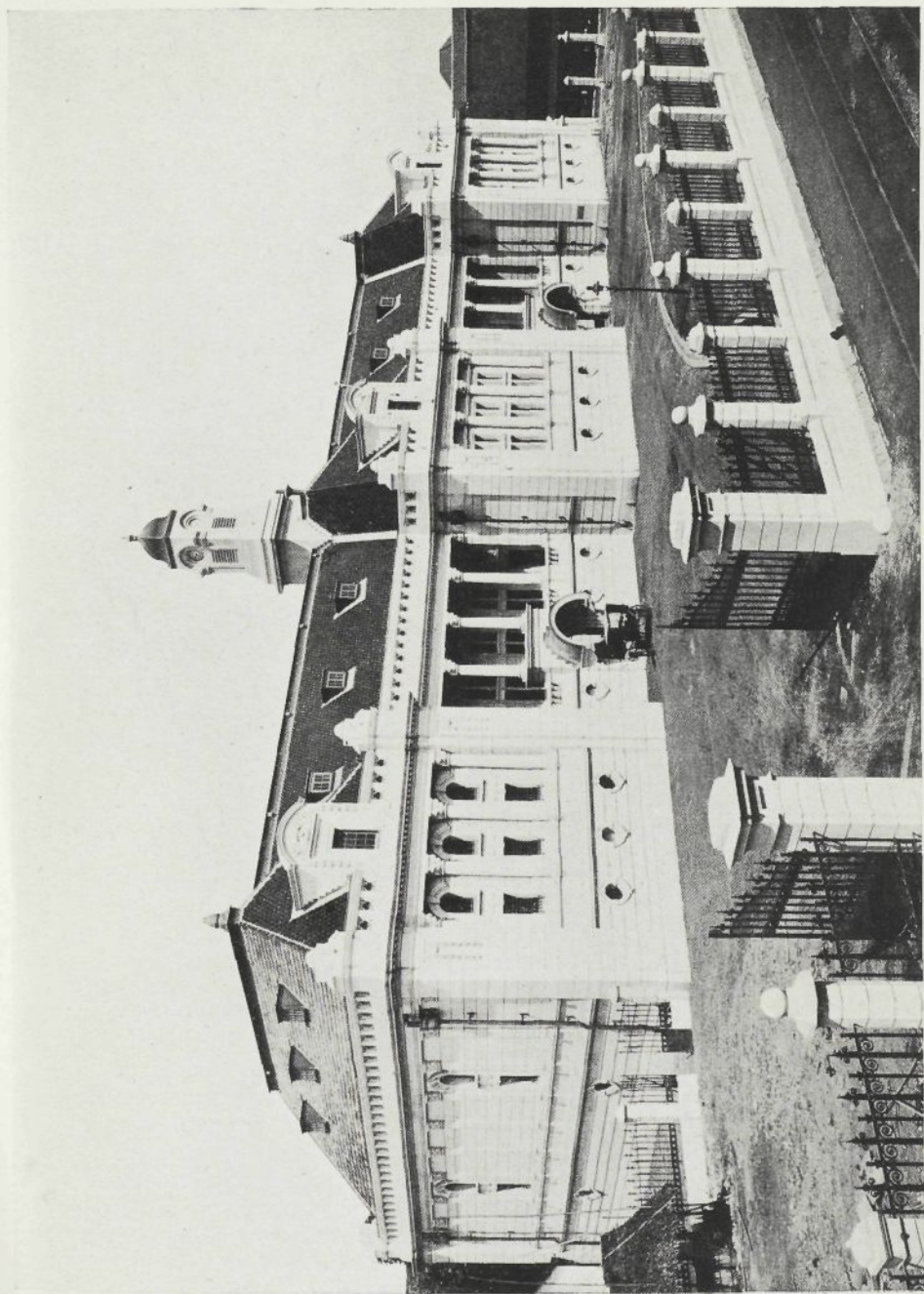
Historic Batavia . . . . ! What a pity it is that so little is left of the old town of Batavia, once, in the 17th and 18th century, known throughout the East as the "Queen City of the Orient". According to the records of the historians and travellers of those days it must have been a really beautiful and even imposing town, well laid-out and well-kept, and it fully deserved the above mentioned poetical appellation. It should be understood however that the character of the town was not at all Oriental. It was truly a Dutch town, with typical Dutch canals and draw-bridges (many of which are still in use) and houses built in the same curious but charming style as those in old Holland. In fact, very little of the old town has been preserved; at present the ancient residences of our forbears are usually occupied by Chinamen of all professions. Furthermore the offices of the European firms are situated in the old town and many of them have built up-to-date office buildings to replace the old residences of a by-gone age. It cannot be denied that historically, present day Batavia is inferior to Manila, the "Pearl of the Orient", where the well-preserved ancient Spanish part of the town (Intramuros) still reminds one forcibly of mediaeval Spain, and recalls to the traveller the forms, now gone to dust, of the Spanish *senoritas*, adventurers and cavaliers who once populated the narrow streets where now the electric cars tell the story of modern civilisation and technical achievement. As far as the preservation of Dutch historical landmarks in the Indies is concerned the Dutch have singularly fallen short and it is especially in the old parts of Batavia that this is evident. Nevertheless, there are still a few places, e.g. along the river, known as Kali Besar (Large River), where one gets a decided impression of being in some forgotten Dutch town along the Zuydersea. This is especially so in the early evening hours, after the offices have been closed, and, with dusk settling on the town, everything is becoming quiet and peaceful. In addition there are a few places and buildings which recall to the mind the early history of the town as well as of the United East India Company, as can be seen from many of the photographs reproduced in this guide: a few of the partly preserved old Dutch houses referred to above, the ancient Portugese church, which is the only one left of the six churches the town once possessed, the building known as Toko Merah, occupied until recently by the Bank of India, and a few others.

It is certainly to be regretted that not even parts of the other churches were protected against destruction or demolition, as some of them must have been rather remarkable from an architectural point of view, which is not exactly the case with the one still in existence. The Portuguese church was built in 1695, and was then located outside the walls of the town. There lived amongst the Dutch population a fairly large Portuguese community which accounts for the fact of there having been two Portuguese churches in old Batavia, the other one being situated within the limits of the town. But there are still a few more relics of the past in old Batavia: the skull of Pieter Erberfeld, which reminds one of the fierce passions which so often blazed in the hearts of our forbears, who went a-roving all through the East, in quest of adventure, but more often for the riches of the Eastern trade. It shows a plaster skull with a spike driven through it on top of a wall, which bears the following inscription: "Uit een verfoeilijke Gedagtenisse tegen den Gestraften Landverraader Pieter Erberfeld zal niemant vermogen te dezer Plaatse te Bouwen, Timmeren, Metselen, of te Planten nu of ten Eenigen Daage. Batavia den 14den April A. 1722", which in modern English reads as follows: "To perpetuate the punishment of the detestable crime of Pieter Erberfeld, a traitor to this country, it is forbidden to build, to carpenter, to work in stone or to plant in this place, now and for ever. Batavia 14th April 1722".

The story can be told in a few words. Pieter Erberfeld was the son of a European and a Javanese woman, and had inherited the property of his father

when the latter died. In 1721 he was supposed to have plotted with the natives to murder the European population and to install himself as ruler of the town. A short time before New Year's Day 1722 the Governor-General Zwaardecroon was informed as to what was being planned and accordingly Erberfeld and others were put to death in the usual cruel fashion of the age. But it must be mentioned that in after years historians have asserted that Erberfeld was not guilty of the crimes he was charged with, and that he fell a victim to the personal animosity of the Governor-General, who coveted part of his property. It is a remarkable coincidence that within a stone's throw from the wall which bears the skull of Pieter Erberfeld and near the entrance to the Portuguese church, now lies the grave stone of this same Governor-General, bearing in sumptuous design his crest and titles. Another old relic, even older than Pieter Erberfeld's skull, is the so-called "Sacred Cannon", which is connected with Batavia's most remote history and holds a special place amongst the legends and stories of the native population. When Coen, the first Governor-General in the service of the United East India Company, was engaged in the struggle with the old native settlement of Jacatra, situated to the South of the first Dutch settlement and fortress, named Batavia, the gun is supposed to have been captured from the Dutch, the latter afterwards recovering it again. However, this is mere tradition, as nothing is definitely known, only from the inscription on the gun one might derive that it was recast from another one, as the same reads: "out of myself reborn" in Latin. According to the natives the wife of a native chieftain, whose marriage was childless notwithstanding the prayers of wife and husband, happened to sit down on this gun, and was then blessed with a child. Since that happened the natives consider the gun as holy, and on holidays, one can often see native women going down to it and depositing round it offerings of strongly perfumed flowers, paper umbrellas, and burning incense, expecting that by doing so their marriage will be blessed with numerous offspring. In the residency of Bantam, to the West of Batavia there is supposed to be a similar cannon. Here the men flock to and bring their offerings. There is a story current amongst the natives that the man who brings these two guns together, will overthrow the power of the Dutch in Java, and liberate the natives from foreign domination. Close to the gun is the so-called Penang Gate, which once formed part of the city's walls. The ugly bronze statues on each side are supposed to portray Mars and Venus. Beyond the gate must have been the site of the old castle of Batavia, but at present all that is left of the old buildings are a number of semi fortified warehouses of the East India Company.

The few relics of old Batavia mentioned above are those usually visited by all tourists, and we beg to refer to pages 21 and following for their exact situation. For tourists who just pass through Batavia it is rather difficult to see more, as it is in the out of way corners of the ancient capital that one, sometimes, comes by chance on some old building or wall, reminiscent of olden days. Such is only in store for the more inquisitive and demands a longer stay to allow for a few days wandering through the town. The great difficulty encountered by those in search of relics of the past in Batavia, lies in the fact that up till quite recently no records have been kept and no attempts were made by the authorities to ascertain the exact situation of streets and buildings mentioned in old volumes. It was not before the 300th anniversary of the town, that the Batavia Society of Arts and Sciences in co-operation with the publishers, G. Kolff & Co., commissioned Dr. R. de Haan, the Government Librarian, to publish the Records of Batavia. This book is very accurate and trustworthy and dispels many wrong notions about the founding and subsequent development of the town. It consists of three volumes, one of which contains a select collection of photographs and reproductions of old prints, and with the help of this book one is able to obtain a correct idea of the former appearance of the town. However, as it is edited in Dutch, the text is of little value to the stranger, but the volume containing the photographs prove useful.



MODERN BATAVIA: THE JAVA BANK BUILDING, DOWN TOWN.

## DESCRIPTIVE GUIDE TO BATAVIA AND WELTEVREDEN.

Batavia, the capital of the Netherlands East Indies, is situated in the Western part of Java, in  $6^{\circ} 8'$  Latitude South and  $106^{\circ} 48'$  Longitude East. The population of the city with the suburb Weltevreden and the adjacent township of Meester=Cornelis, is 306,308, of which 29,373 are Europeans.

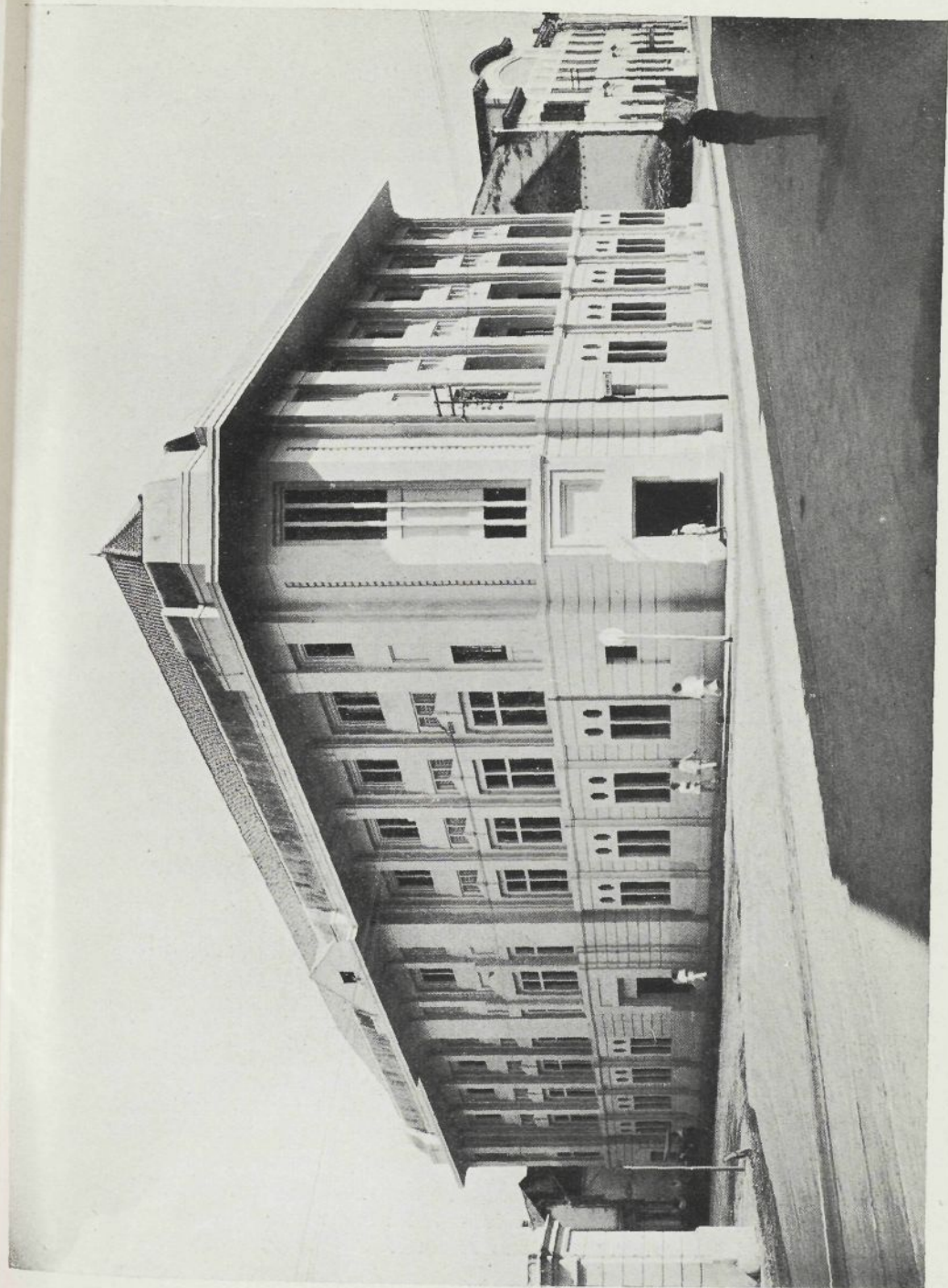
It is the town where most tourists begin their tour through Java, and is distanced some 532 nautical miles from Singapore. This distance is traversed by steamers in some forty hours, the route from Singapore leading past the Rhio Archipelago and through the Straits of Bangka, which divides the Islands of Banka and Billiton, both well known for their extensive tin mining. During the voyage one crosses the equator.

*Arrival.* Steamers for Batavia arrive at the harbour called Tandjong Priok, situated at a distance of some 6 miles from the town. This harbour was opened in 1886 after nine years work and originally consisted of an outer basin and an inner basin. At present it has three inner basins.

The entrance to the outer basin from the sea is formed by two long breakwaters, each about 6000' long. The basin itself has a depth of some 35'. The inner basins are about 35000' long and 560' wide and have each two quays, along which sheds, warehouses and some offices of shipping companies are located. The harbourworks are provided with the most modern equipment, including electric cranes, a railway system along the quays and other up-to-date material.

The first inner basin is mostly used by steamers of the Royal Packet Navigation Company, the Burns Philp Line and smaller steamers of foreign nationality. The second inner basin is at present in use for the Netherlands Royal Mail liners and larger steamers of foreign nationality. As most tourists coming to Java travel via Singapore, they land in the first inner basin.

The station at Tandjong Priok is situated to the East of the harbour basins, and since 1925 electric trains are run to Weltevreden and Mr. Cornelis. As might be expected the electric system will be considerably extended within the next few years. Motor cars are also always in evidence on the arrival of ships, the charges for a car to the upper town, Weltevreden being about Fl. 7.50, although sometimes a car may be had at a little lower figure. A money exchange is found on the premises of the Batavia Veem near the second inner basin. The post and telegraph office is located near the entrance to the first inner basin, and the harbour masters office (where the money paid for landing permits is refunded) on the quay of the 2nd inner basin. A public telephone stand is on the platform of the railway station.



MODERN BATAVIA: THE BUILDING OF THE NETHERLANDS INDIA DISCOUNT BANK, DOWN TOWN.

The distance to Weltevreden is covered by rail in less than 20 minutes. Trains leave Tandjong Priok for Batavia and Weltevreden about every half hour, except on Sundays and public holidays when some of the trains are suspended. By motor car it takes about half an hour to reach one of the hotels.

*Customhouse.* The examination rooms of the customs are located in the sheds on the quays near the entrances to the basins, the one of the first inner basin being located near the main entrance. Luggage is brought from the steamers by coolies to the examination rooms and after being passed, brought by the same coolies to the motor car.

Every incoming steamer is met by the porters of the different hotels, to whom luggage may be given in charge, whilst they may also be charged with the buying of railway tickets, the ordering of motor cars, forwarding of luggage by train, and the like. Most of these porters speak a little English.

*The township of Batavia.* The town of Batavia was founded by Jan Pietersz. Coen in 1619, on the banks of the river Tjiliwung near the seashore. In the beginning the town extended to the South along the banks of the Tjiliwung and it was only during the latter years of the 19th century and the first half of the last century that Europeans settled in the quarters which are at present known by the name of Weltevreden. This was for a good deal on account of the old town being rather swampy and as a result, unhealthy, and at present no Europeans live in the old town. To-day only the offices, banks, warehouses and different mercantile houses are located in Old Batavia and the population consists chiefly of Chinese and Natives.

The business hours of the business houses in Batavia are from 9 o'clock a.m. till 4 or 5 o'clock p.m. Batavia may at present be divided in two different districts viz. Old Batavia and Weltevreden, which are different from each other in every respect.

*Old Batavia:* the Benedenstad or down-town city as it is usually called, antedates Weltevreden by some two centuries. It was built on a site near the sea in old Dutch style intersected by canals and with narrow streets. Most of the buildings of olden times have gone, but a few like the Town Hall, the Portuguese Church, the Bank of India and the house formerly occupied by the mining department, still show the style of building then in use. Old Batavia contains very few relics of the early days, but it is a quaint and delightfully picturesque town, its canals adding much to its charm to the stranger.

The main road which joins the old and the new city runs along the banks of the river Tjiliwung and is called Molenvliet (Mill stream). Starting from its beginning near the Harmonie Club, one first sees to



TANDJONG PRIOK: HARBOUR VIEW IN THE SECOND INNER BASIN WITH A DUTCH MAIL LINER ALONG THE QUAY.

part consists of cast iron and it is quite a good specimen of the tomb stones made in those times. In the church itself is a rather fine pulpit and on the walls hang a number of escutcheons, one of these bearing a reference to the founding of the church.

Going on from the church a few yards up Jacatra road, one's attention is drawn to a section of an old wall, which is surmounted by a white washed human skull, transfixd by a spear point. A tablet below the skull bears an explanation in Dutch and Javanese, stating that in detested memory of the traitor Peter Erberfeld, building or planting in this place is forbidden for now and all time to come. (See page 17).

But besides these few remains of early days, Town Hall, Penang Gate, sacred cannon, church and skull, there are hardly any places of historic interest left. The number of old mansions, once the houses of the rich merchants, are transformed into banking, shipping and other commercial offices, although, sometimes in an altogether unexpected corner one may find traces of former splendour in carved balustrade and stairways, fine doors and wonderful old knockers.

Driving through the present Chinese quarters of old Batavia, a most picturesque part of the town, looking with all its canals like a Chinese Venice, and on to Kali Besar, the river, with on both sides the European mercantile houses and offices, one reaches at the end of this street an old Dutch drawbridge and on entering the street to which the bridge gives access, finds at the end of it again the Penang Gate already mentioned.

*Weltevreden*, the upper town is the modern part of Batavia. It is the district where most European residents live and the government offices, hotels, clubs and shops are located. It may aptly be called a garden city with its broad roads, large squares, abundance of shade trees, large public buildings and hundreds of delightful houses and bungalows, built far back from the roads and surrounded by spacious gardens and lawns. A pity it is however that so little is done in the way of garden architecture and landscape gardening.

The name *Weltevreden* means well-content and the town is altogether different from the towns found in other parts of the Far East. Sightseeing in the town is best done either in the morning or in the late afternoon as during midday the heat is rather a drawback. The principal streets in the shopping district are *Rijswijk* and *Noordwijk*, on either side of the river *Tjiliwung*. Another place of interest is the *Koningsplein* (a large square to the South of *Rijswijk* and connected with that street by two main roads called *Gang Pool* and *Gang Secretarie*). On the North side of this square one sees the Palace of the Governor-General, which however is only sometimes occupied, as the greater part of the year H. E. lives in his palace at *Buitenzorg* in the famous Botanical Gardens. Opposite the palace we see the Telephone Offices, one of the newest buildings of the town. Alongside are the recreation grounds called *Deca Park* and a little



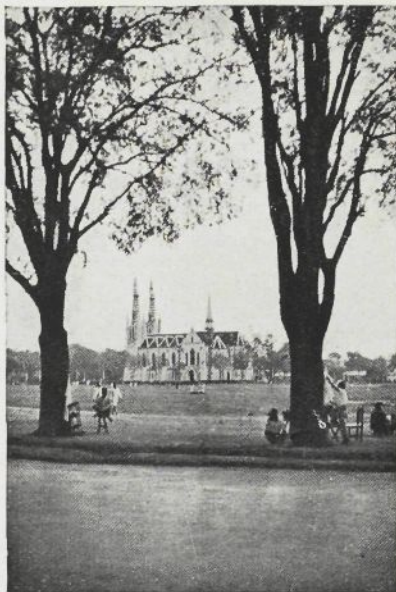
WELTEVREDEN: THE HOTEL DES INDES, WITH THE FAMOUS BANYAN TREES IN THE FOREGROUND.

further on, on the opposite side, the back entrance of the Department of the Civil Service which has its main entrance in Rijswijk. On the West side one sees the Hotel Koningsplein, the college for civil servants, the Judicial University and the museum, and on the North side the mansion of the Governor, the building of the Royal Natural Historical Society, and many imposing private mansions. On the East side are the central Railway station and more private mansions. Adjoining the square to the North East lies a small park called Wilhelmina park. In the centre of this park with the Tjiliwung river for moat, is an old fort called Prins Hendrik, which at present is used for arsenal. To the South of this park is a street called Willemslaan which forms the connection of the Koningsplein with the Waterloo Plein, another square, smaller than the Koningsplein, in the midst of which one sees a column erected in 1828 in commemoration of the battle of Waterloo.

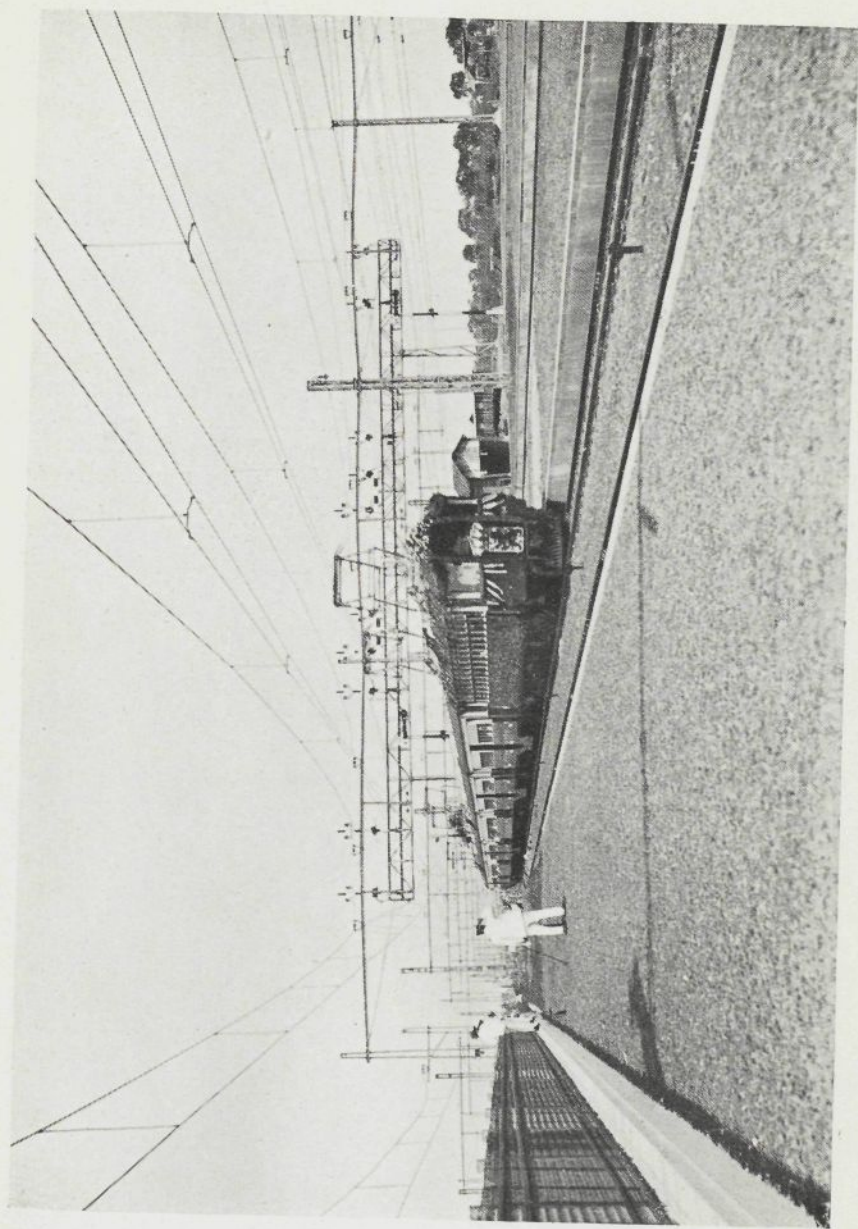
At the end of Willemslaan, is a monument to the memory of General Michiels, who was killed in the expedition to Bali in 1849.

The Waterlooplein had originally only officers bungalows on three sides, but at present the Catholic Cathedral, conspicuous by its two slender open worked iron towers, stands on the North side. Next to this is the high school for girls which forms part of the convent and on the East side of the square one passes first the masonic lodge, then the High Court of Justice and next to this the large building which originally was intended as a palace for the Governor-General but which never was used as such. At present different Government offices are located here. Next to this palace is the Concordia Club (Army & Navy), in the midst of a small but attractive and well-shaded garden.

Amongst the other principal streets or rather roads in Weltevreden are: Kramat, the main street leading to Meester-Cornelis and on to Buitenzorg, and Tanah Abang, which is a continuation of Rijswijk Street. At the end of a side street of Tanah Abang lies the European cemetery, and at the entrance one sees a few old grave stones, decorated with armorial bearings, calling one back to the times of the East India Company. This cemetery contains the grave of Lady Raffles, the wife of Sir Stamford



THE STEEL SPIRES OF THE CATHEDRAL AT WELTEVREDEN ARE MOST CONSPICUOUS IN THE TOWN'S SKY LINE.



THE DECORATED ELECTRIC PASSENGER TRAIN WHICH CARRIED THE GUESTS TO THE RECEPTION HELD  
BY THE J. G. RAILWAYS OFF TO PRIOK ON APRIL 5th OF 1925.

Raffles, the British Governor-General during the period of the English occupation (1810). There is furthermore Kebon Siri, which connects Tanah Abang with Menteng, and Pasar Baroe, the Chinese part of Weltevreden, which is well worth a visit on account of its many shops. At the end of it is a covered fruitmarket.

*Government Offices.* The principal Government offices in Weltevreden are: the Department of the Civil Service in Rijswijk, Department of Education and the one of Finance in the palace on Waterloo Plein, Customs in Schoolweg, Department of Public Works in Molenvliet and the Naval Department in Goenoeng Sahari.

*Means of communication.* An electric and a steamtram keep up the connection between Meester Cornelis and Weltevreden and Batavia, only the steamtram running to Meester Cornelis. In addition to these, three lines of Railways connect Weltevreden at different points with Batavia and Tandjong Priok. An electric train service (every 15 minutes) connects Tandjong Priok with Meester Cornelis via Kemajoran, Pasar Senen and Kramat. The continuation of this line to Weltevreden station and Batavia is at present in course of construction and will probably be opened within the next few months.

Of the Railways lines, the Bantam Line runs along the outskirts of the city to the West, starting at Batavia South, and passing the stations of Angke, Doeri and Tanah Abang, all situated in the township of Batavia.

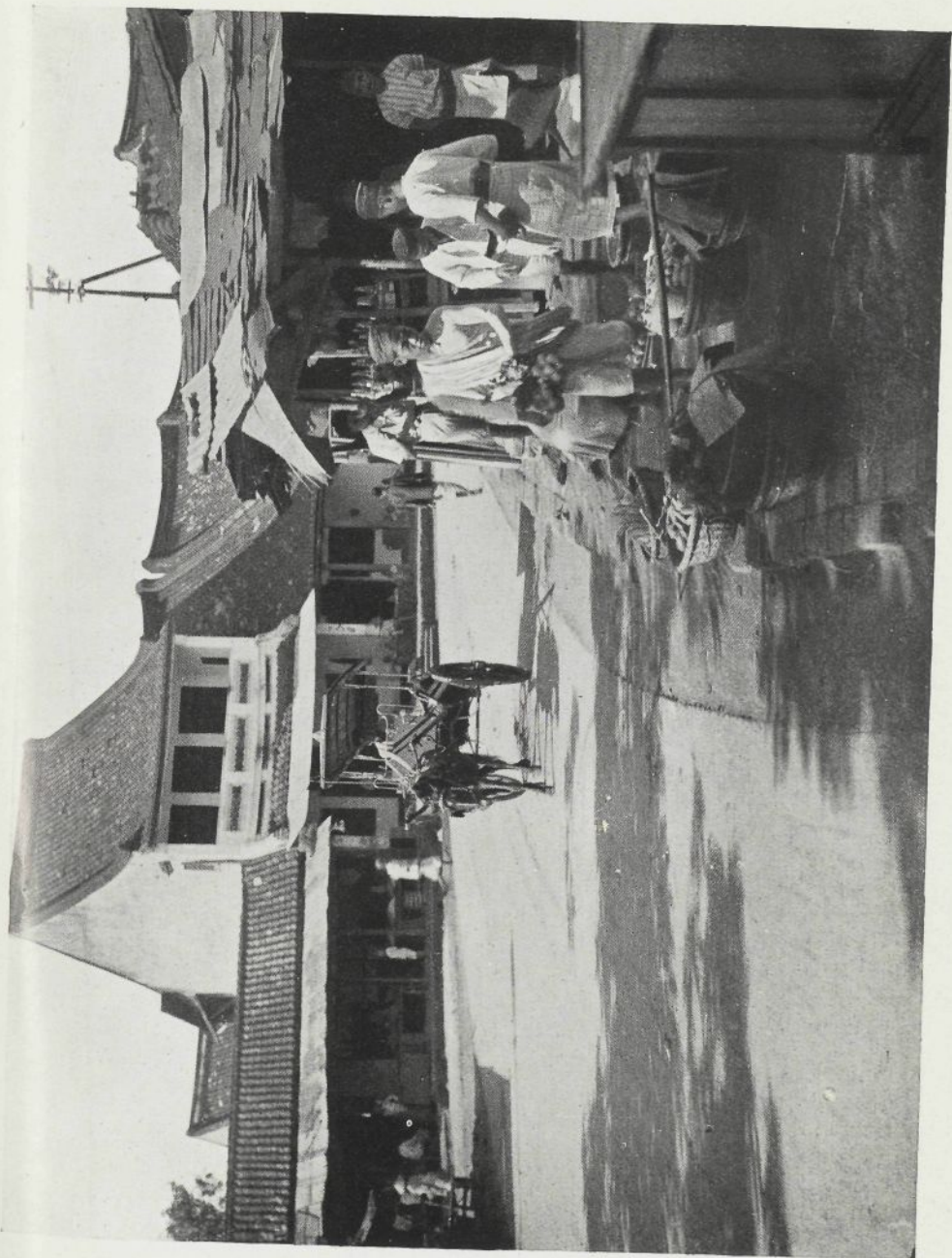
The railway line to Bandoeng via Krawang starts at Batavia, and has for stations Weltevreden, Mangarai and Meester=

Cornelis,  
all within the  
boundaries of  
the townships  
of Batavia=  
Meester=

Cornelis.  
The line to  
Buitenzorg  
starts at Bata=  
via North, and  
passes within  
the limits of  
Batavia the  
stations of  
Noordwijk,  
Weltevreden,



THE FAMOUS SACRED CANNON.



NATIVE LIFE IN 20th CENTURY BATAVIA; NEAR TANAH ABANG HILL IN WELTEVREDEN.

(Koningsplein), Kebon Sirih, Dierentuin, Pegangsaan and Mangarai. The latter line is at present the main line, starting from Weltevreden station; the connection with the line to Bandoeng and Cheribon is made by a branch line from the station Mangarai to Meester=Cornelis.

The latter line runs nearly in a straight line through the heart of Weltevreden. The Bantam line is connected with the other two by a branch line which runs from Tanah Abang to Kramat.

*The line of the steamtram* has as starting point the Kasteel plein in Old Batavia and runs from there to the townhall and on through the Binnen Nieuwpoort Street, past the railway station Batavia South through the Buiten Nieuwpoort Street to Glodok square. It then proceeds along Molenvliet West, to the Harmonie Club, turning to the left and running along Rijswijk and Sluisbrug to the Schoolweg, goes on to Waterloo Plein, where it crosses the track of the electric tram. From Waterloo Plein it passes the Chinese quarters at Senen, to enter Kramat. A little farther it reaches the Station Kramat, which is the terminal of the Batavia=Weltevreden tramline.

The line is continued from Kramat station to Meester=Cornelis and has its terminal at the end of Kerkstreet.

Trams run about every 10 minutes, fare 1st class Batavia=Kramat 20 cents, Kramat=Meester=Cornelis 20 cents.

The daily service is kept up between the following hours:

From Kasteelplein to Kramat from 6 a.m. to 6.40 p.m.

From Kramat to Kasteelplein from 5.10 a.m. to 6 p.m.

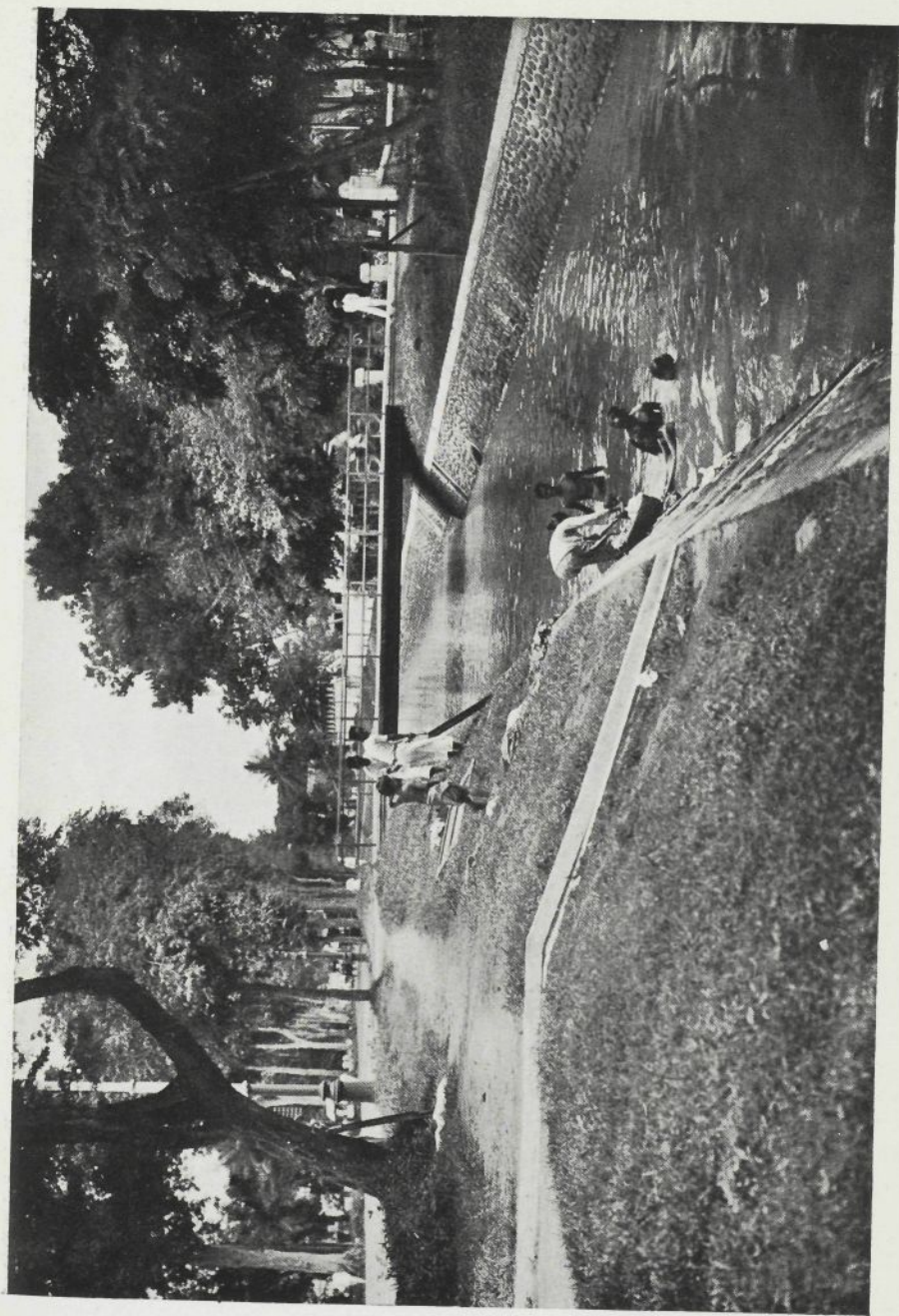
From Kramat to Meester=Cornelis from 5.20 a.m. to 6.20 p.m.

From Meester=Cornelis to Kramat from 6 a.m. to 6.50 p.m.

*The electric tram line* starts at the Penang Gate in Old Batavia. From there it runs along Kali Besar East, past the station Batavia South, the Portuguese Church and Pieter Erberfelds skull, along the Jacatra Road and Gunung Sari to run through the Chinese quarters at Pasar Senen to Menteng, crossing Kramat on the way. From Menteng it runs along Kampong Lima (Tamarindelaan), Tanah Abang and Rijswijk street to the terminal near the Harmonie Club.

There are two branch lines, one running from the terminal at the Harmonie Club via Rijswijk Street and Koningsplein North and East to Menteng, where it connects with the main line from Batavia, and a branch line running from Koningsplein East along the Willemslaan=Waterlooplein and the Vrymetselaarsweg to Gunung Sahari, thus connecting the first branch line with the main line at Gunung Sahari. A drive with the electric tram is recommended to those interested in native life as it runs through parts of the native "Kampongs", where one cannot enter with a motorcar.

Trams run about every 10 minutes. The tariff is for each section 20 cents, the line being divided in the following sections Harmonie Club —



NATIVE LIFE IN 20th CENTURY BATAVIA; VIEW OF TANAH ABANG.

Menteng  
either via  
Koningsplein  
or Kampong  
Lima. Men-  
teng — Old  
Batavia.

Koningsplein  
— Goenoeng  
Sahari via Wa-  
terloo Plein.

Hours of the  
daily service  
are as follows:

From Batavia  
to Harmonie  
Club from

5.50 a.m. to 5.40 p.m. From the Harmonie Club to Batavia from 5.50  
a.m. to 5.25 p.m.



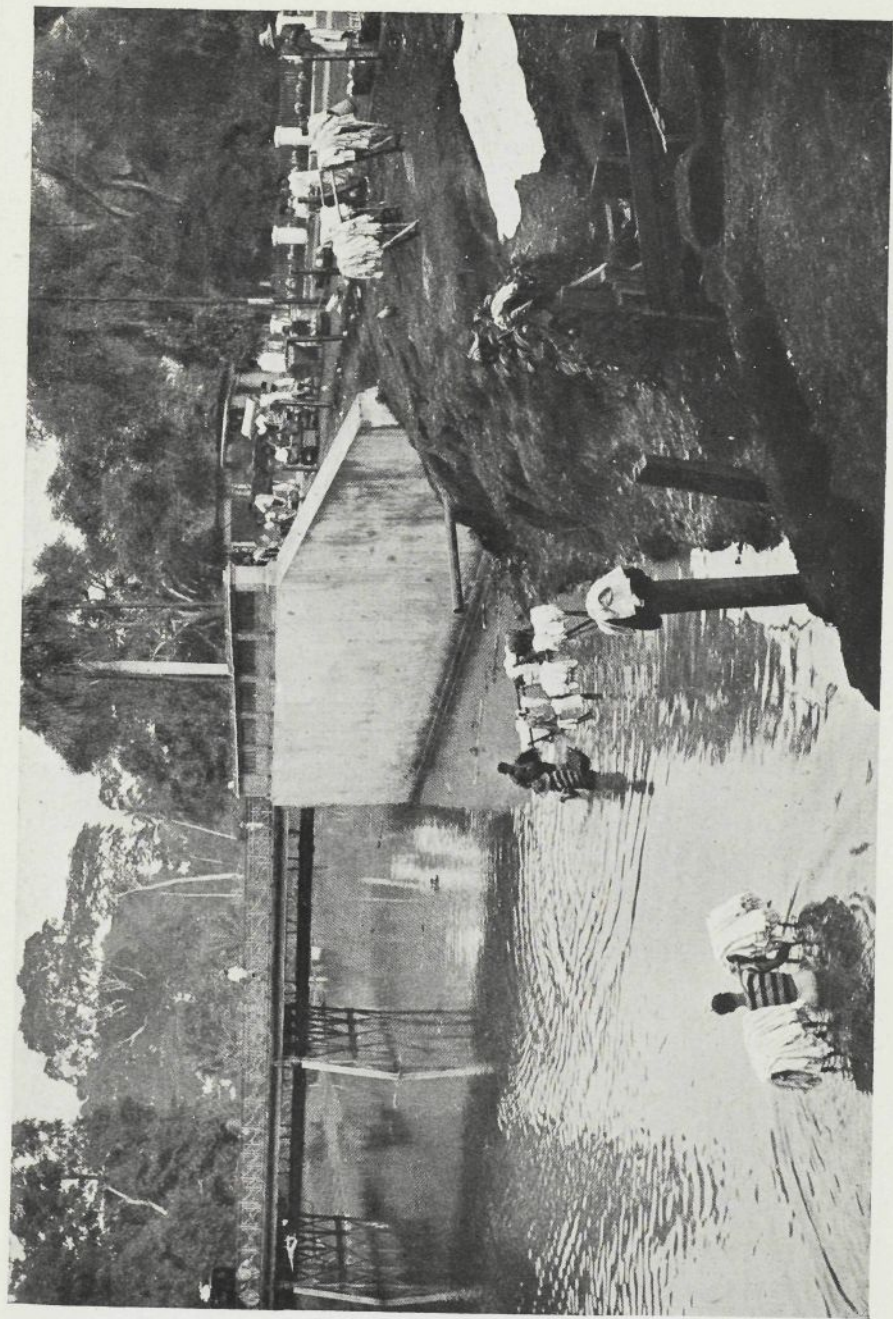
THE WANDERING NATIVE RESTAURANT.

*Motor cars.* These may be hired from the hotels and autogarages. Tariff for rides in town from Fl. 5.— to Fl. 6.— an hour. For tours in the country arrangements must be made in advance with a garage. If a motor car is required it is advisable not to call one from the street, but to ring up a reliable garage.

*Garry's.* There are two kinds of garry's in Batavia viz. two wheeled one' called sado's, where one sits with one's back to the driver, and fourwheeled, with two horses. Tariff for the first named Fl. 1.— an hour with a minimum of 25 cents for the first quarter of an hour. For the last named the tariff is Fl. 1.50 an hour, minimum fare 50 cents.

Of all the places of interest to be visited in Batavia, the museum takes the first place. It has a wonderful collection of images from Buddhistic and Hindu times, a large ethnographical collection of objects relative to the religious and family life in all parts of the Netherlands East Indies, wonderful gold and silver work from Bali and other Islands, and beautiful weapons, in fact a visit to the museum gives one a most instructive and complete idea of what is to be found in the East Indian Archipelago. For those interested in native life a visit on a Sunday morning can be recommended, as then the museum is full of native visitors of all classes. But to view the objects one should of course choose a weekday.

Hours daily from 9 — 2. Sundays 9 — 12, closed on Mondays.



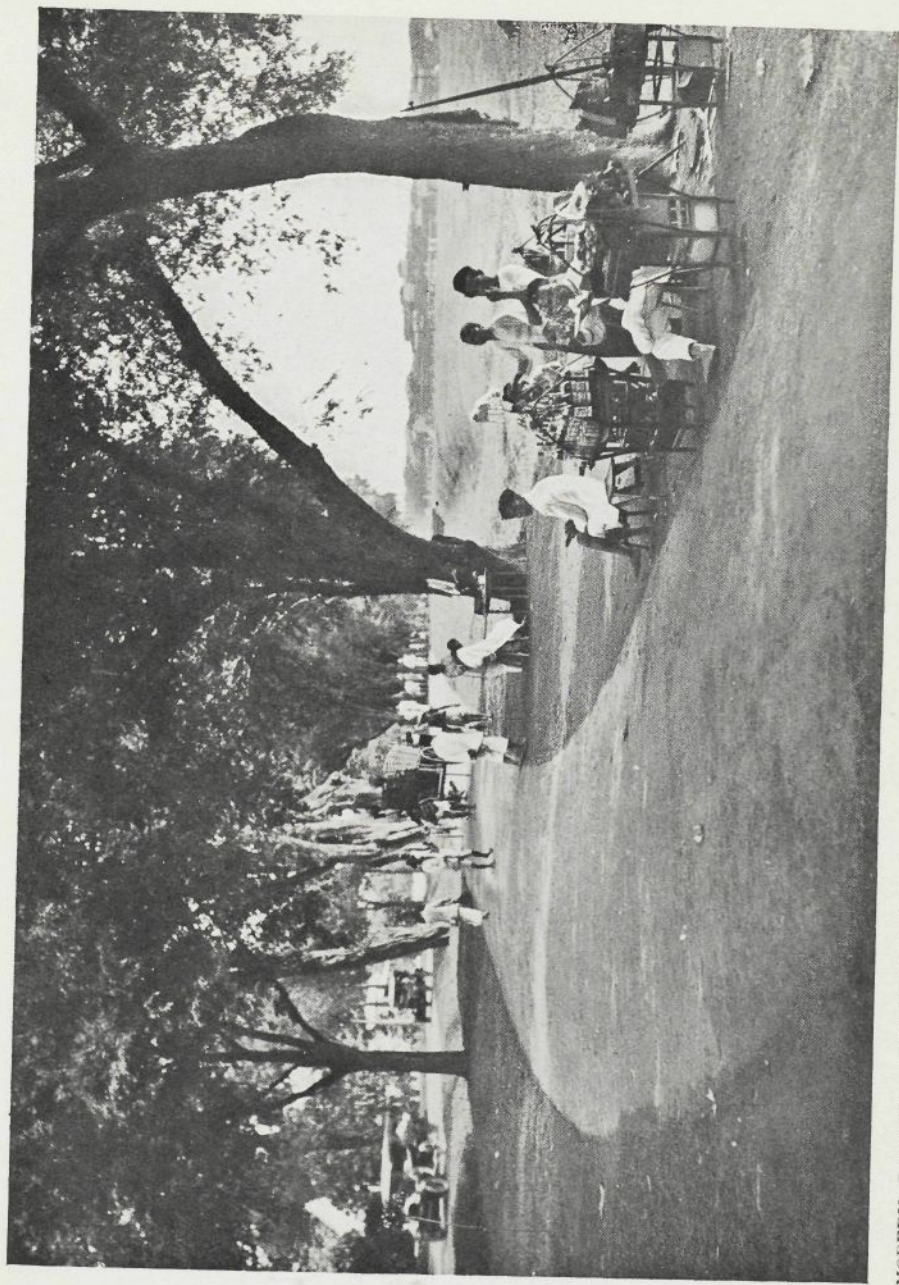
NATIVE LIFE IN 20th CENTURY BATAVIA: THE OPEN AIR LAUNDRY.

The newer parts of Weltevreden, called New Gondangdia and New Menteng are situated to the South of Koningsplein and West of Tjikini Road, which is the prolongation of Koningsplein East and Parapatan — Menteng. It is a well laid out suburb with beautiful bungalows and a drive through that part of the town is well worth while.

When taking a drive (for particulars of the various buildings see page 28 and 29) through the upper town the route followed usually leads, when starting from the Harmonie Club, along Rijswijk or Noordwijk to the railway crossing at Sluisbrug. To the left are the booking offices of the principal Dutch steamships companies, whilst the Head-Office of the K. P. M. is located (one of the largest office buildings in Java) opposite the Railway station on the Koningsplein. Going on along Schoolweg one passes to the right the General Post Office and the theatre. Here the road turns to the right and leads to Waterloo Plein. Going straight on one comes to Pasar Senen, the bungalows on the right being occupied by army officers whilst to the left Chinese quarters are located. Crossing a bridge the broad Kramat Street is entered which is the main road leading from Batavia to Buitenzorg and Bandoeng. First the tramway station is seen on the left and continuing, fine European residences are situated on both sides.

Just after crossing the railway track, the Government opium factory is passed to the right and a little further on the large imposing building of the S.T.O.V.I.A. (medical college for natives). From there the road leads past the Roman Catholic hospital St. Carolus (to the left) and the High School for European children, (to the right). Just a little beyond the latter building a small road to the right is followed which leads to the suburbs of Menteng and Gondangdia. Driving through these parts one leaves the suburbs at the entree of New Gondangdia entering there the Menteng street. This street leads via Parapattan to the Koningsplein and turning to the left one drives along the South side of the square. Turning to the right the West side of the square is followed and after turning to the left the road leads to Rijswijk Street at the end of which the Harmonie Club is located.

If the visitor to Batavia has plenty of time at his disposal a trip may be made to the village of Karet on the outskirts of the town. The trip which should be made by motor car leads via Tanah Abang and Paal Merah; the attraction of this small village is the batik industry and whilst the work produced here cannot compare with that from Mid-Java, it is well worth a visit. The return journey has to be made by the same route.



MODERN BATAVIA: ALONG THE KING'S SQUARE AT WELTEVREDEN, WANDERING NATIVE RESTAURANTS TO THE RIGHT.

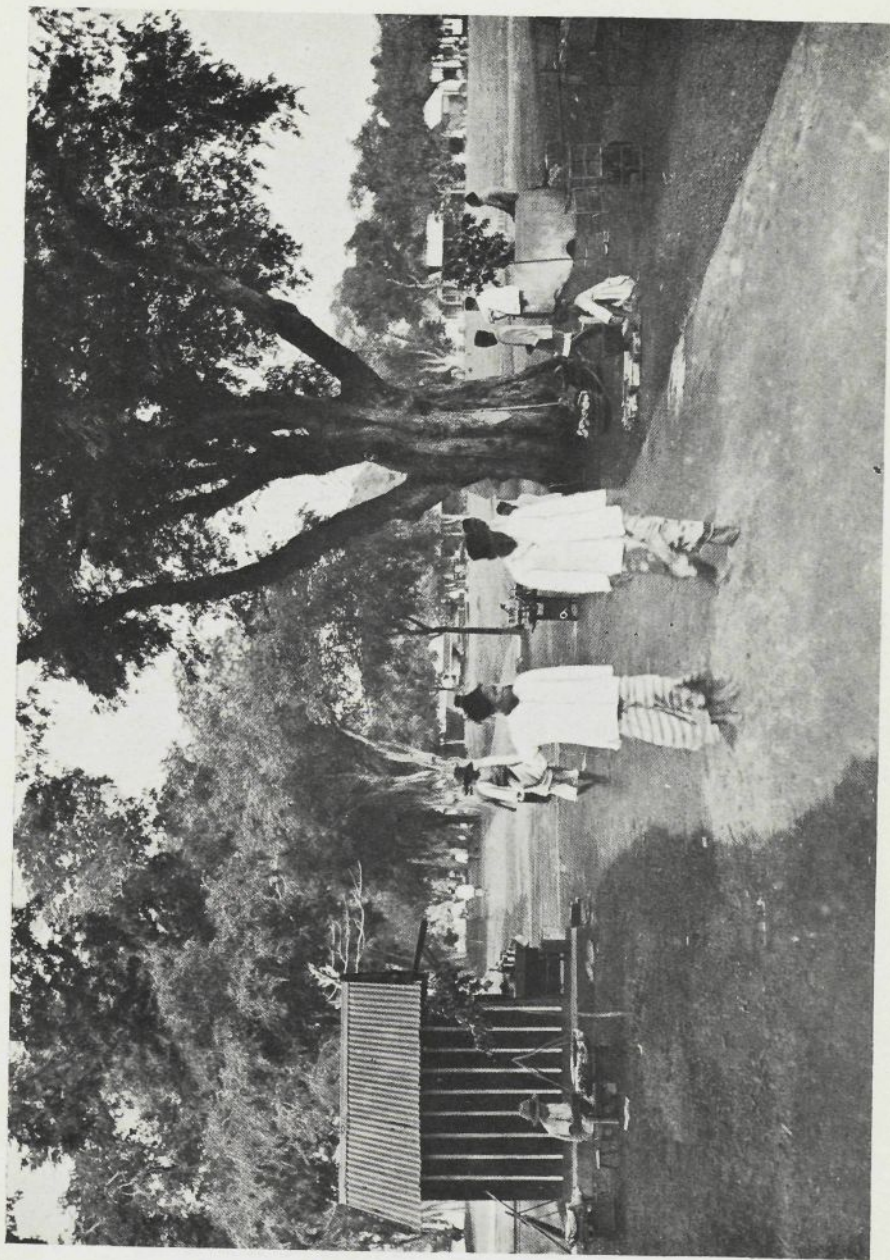
derant place. Since some time the native population have as their representative a native regent, which was formerly not the case, but it is doubtful if he is closely in touch with the people. The larger part of Batavia's population is of a floating nature, there is no or hardly any tradition that binds the larger number of the natives to Batavia. In the native quarters the administration rests with village chiefs, but apart from them there are no native Batavia families who are rooted in the town from former times up to the present. The higher native officials are for the larger number of Javanese or Sundanese extraction, and have their birthplace in quite another part of the island. The same



WITH A SMILE THAT WAS HAPPY AND BLAND.

applies to the Resident of Batavia. In addition to the native population hailing from all parts of Java and the other islands of the Archipelago there is a large Chinese and a smaller Arabian Community in Batavia. The Chinese take an important place in the economical life of the town and are active in the most humble as well as in the most elevated stages of life. As artisans and store keepers, generally speaking as retail dealers, they have become indispensable. In contrast to other cities in the East, they are rarely employed as servants, as the natives have almost a monopoly in this field of human activity.

The Arabs are also an important class of the population and the larger number of residences in the upper town are owned and let by Arabs. They are held in high esteem by the natives on account of religious traditions, and for their reputed wealth.

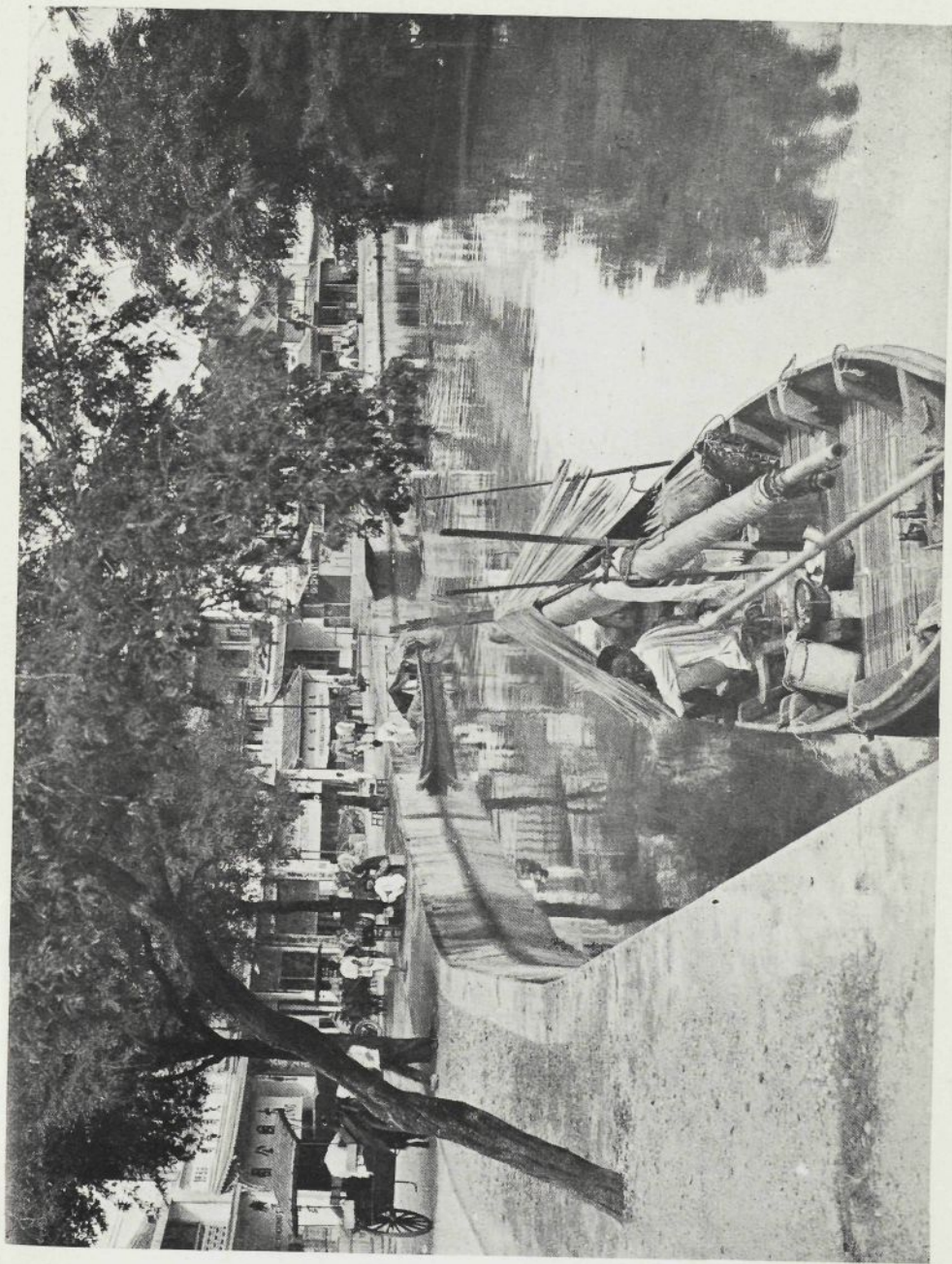


MODERN BATAVIA; ROUND THE KING SQUARE, WELTEVREIDEN.

To see native life in Batavia as it really is, and to obtain an idea of the living conditions of the people, one has to go away from the main thoroughfares, and wander round the native quarters, or kampongs, and those genuinely interested in native life, should not fail to reserve a morning or late afternoon for this purpose. In the evening one might also pay a visit to a native place of amusement, witness a so-called "Stamboel", or opera (a Malay version of some European production) although the film is steadily becoming more popular and Malay plays more scarce. To witness a wayang performance in Batavia is not so very easy, as this is a special Javanese art, but still, if one knows a few people in the town, with some good luck it might be arranged. Compared with what one sees in Mid-Java the performances are however rather poor. Besides, the majority of Batavia natives prefer a "Stamboel" or picture show.

There is a tendency with many Europeans to rate the native population of the larger towns in Java very low, but we think that this opinion is not entirely correct. The native in the town misses the humbleness and submissiveness of the country people, and he has adopted many habits of the European, and at the present to some extent also affects his dress, when he can afford it. Still, his inborn feeling for the propriety of things, of what should be done and what should not be done, has not been lost, and when dealt with in a straightforward and correct way, he is a very amenable and polite fellow. And we are convinced that those really interested in the life of other races, and free from prejudices, will have no trouble whatever when they come in touch with him.





CANAL LIFE IN OLD BATAVIA. TO THE LEFT CHINESE HOUSES AND SHOPS.

PRACTICAL INFORMATION  
FOR THE VISITOR.

**The Official Tourist Bureau.** The Official Tourist Bureau, of Weltevreden, Java, is the only Official Tourist Bureau in the Netherlands Indies. It is a non-profit institution, and has no relations whatever with private concerns.

Established for the promotion of the tourist traffic it renders every possible service to the travelling public by the publishing of time tables, guide books etc. and the supplying of information as regards the best means of travel, points of interest and the preparing of detailed itineraries. As far as possible everything is done absolutely free of any charge. All inquiries will be gladly answered, either verbally or by letter, but attention is drawn to the fact that the Bureau is not a business concern, and therefore cannot undertake the ordering of hotel rooms, the engaging of guides, the forwarding of luggage, exchange of money etc.

*Head Office of the Official Tourist Bureau :*

Weltevreden Java, Noordwijk 36, telephone: Weltevreden 443; cable address "Touring" Weltevreden.

Office hours 8.30 a.m. to 1 p.m. and from 5 to 7 p.m., on weekdays. Those wishing to obtain information when the office is closed are requested to ring up 1470 Weltevreden.

*Branch Offices and Representatives:*  
*For the United States and Canada.*

The Secretaris of the Holland-American Chamber of Commerce, Mills Bldg. Montgomery Street, San Francisco.

*For the Philippine Islands.*

Messrs. Meerkamp & Co., Manila P. I.

*For Japan.*

The Agent, Java China Japan Line, Kobe.

*For Shanghai.*

Messrs. the Holland China Trading Co.

*For Hongkong.*

The Agent, Java China Japan Line.

*For French Indo China.*

Messrs. Diethelm & Co., Saigon.

*For Siam.*

Messrs. Diethelm & Co., Bangkok.

*For the Straits Settl. and the Federated Malay States.*

The Agent, Royal Packet Navigation Co., Singapore and Penang.

*For Burma.*

Messrs. Trading Co., Late Hegt & Co., Rangoon.

*For British India.*

The Agent of the Java Bengal Line, Calcutta.

*For Ceylon.*

Messrs. Aitken, Spence & Co., Colombo.

*For Australia and New Zealand.*

The representative of the Royal Packet Navigation Co., 56 Pitt Street, Sydney.

Major F. H. Wright, Dutch Consul., 21 Queenstreet, Melbourne.

*For Europe.*

The Official Tourist Bureau of Holland, 30 Hooze Nieuwstraat, The Hague.

From the above named addresses pamphlets and information about the Netherlands East Indies may be obtained and in addition from all the Oriental and Australasian Offices of Messrs. Thos. Cook & Son, the offices of the Japan Tourist Bureau and the Offices of the Travel Department of the American Express Company.

*Branch Offices in Neth. India.*

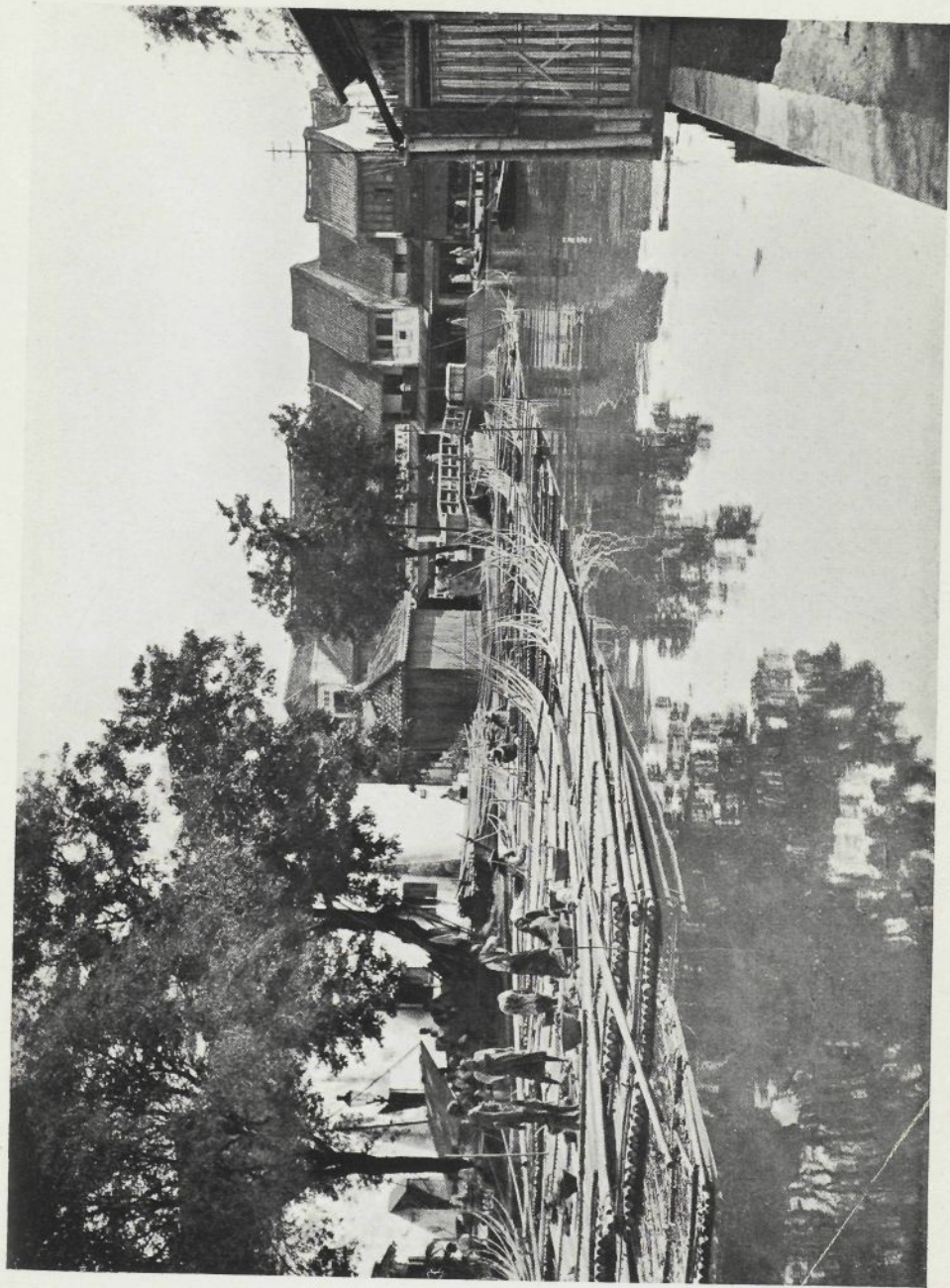
Sourabaya, Boeleleng, (Bali) and Medan.

The Agent, the Royal Packet Navigation Co.

Representative at Djocjakarta :

Miss S. Gobee, Malioboro.

**Weights and Measures.** In addition to the weights and measures of the Metric System various native measures are in use. The principal weights and measures with their English equivalents are the following :



CANAL LIFE IN OLD BATAVIA CHINESE HOUSES IN THE BACKGROUND.

- 1 Picul = 61.76 Kilogrammes = 136.23 lbs.
- 1 katti = 0.01 picul = 0.62 Kilogrammes = 1.36 lbs.
- 1 gantang = 0.10 picul = 6.18 Kilogrammes = 13.62 lbs.
- Of the principal measures we mention:
- 1 Meter = 3.281 feet.
- 1 paal = 1,507 K.M. = 0.9433 E. miles.
- 1 Kilometre = 0.5214 E. miles.
- 1 bouw = 7,096 M<sup>2</sup> = 1.753 acres.

**Currency.** The currency and coins in use in the Netherlands East Indies are similar to those in Holland, with the exception of the coins of less value than 50 cents, to wit those of 25, 10, 5, 2½, 1 and ½ cent, and the banknotes.

Standard coin is the gold coin of Fl. 10.—, which is rarely seen. Further there is a gold coin of Fl. 5.— and silver coins of Fl. 2.50 (rijksdaalder), Fl. 1.— (guilder or florin), Fl. 0.50 (half a guilder), 25 cents (kwartje), 10 cents (dubbeltje), a nickel coin of 5 cents, and copper coins of 2½ cents, 1 cent and half a cent.

The banknotes in circulation are 5.—, 10.—, 20.—, 25.—, 30.—, 40.—, 50.—, 100.—, 200.—, 300.—, 500.— and 1000.—.

There are treasury notes of Fl. 1.— and Fl. 2.50.

**Nominal value of** 1 guilder =  
**Netherlands East** Fl. 1.—, =  
**Indian money in** \$ 0.40 Am.  
**foreign money.** gold = 1/8  
 Engl. = frcs.

2.— French, Swiss and Belgian =  
 Lire 2.— Italian = Peseta 2.— Spain  
 = mark 1.66 German = \$ 0.71½  
 Singapore = \$ 0.38 Mex. = Yen 0.80  
 Jap. = 16½ Annas British India.

**Railway Station Tandjong=Priok,**  
**Stations.** electric and ordinary  
 trains.

Station Koningsplein, Weltevreden.  
 Station Sawah Besar, "  
 Station Noordwijk, "  
 Station Kramat, "  
 Station Senen, "

Station Kemajoran, Weltevreden.  
 Station Tanah Abang, "  
 Station Kebon Sirih, "  
 Station Pegangsaan, "  
 Station Dierentuin, "  
 Station Mr. Cornelis, Mr. Cornelis.  
 Station Manggarai, "  
 Station Salemba, "  
 Station Batavia North., Batavia.

**Hospitals.** Central Civil Hospital  
 adjacent to the School  
 for Native Doctors.

(Government Institution).

Ziekeninrichting Tjikini.

(Protestant).

Hospital St. Carolus (Roman Ca-  
 tholic).

Military Hospital.

**Bookstores.** G. Kolff & Co.  
 Noordwijk No. 13.

Boekh. Visser & Co. Rijswijk 4.

Gebr. Graauw. Rijswijkstr. No. 5a.

Javasche Boekhandel. Rijswijk 2.

Boekhandel De Unie. Binnen

Nieuw Poortstraat.

Obuz, (second hand bookstore).

Gang Pasar Baroe 7.

Minerva (Theosophical). Konings-  
 plein West 17.

**Post and Tele-** General Post Of-  
**graph Offices.** fice, Weltevreden,  
 Postweg.

Post and Tel. Office Batavia. Bin-  
 nen Nieuwpoortstraat.

Post and Tel. Office, Gondangdia.

Post and Telegraph Office, Tanah  
 Abang.

Post and Telegraph Office Mr.  
 Cornelis. Matramanweg 88.

Post and Tel. Office Kramat.

**Newspapers and Daily.**

**Periodicals.** Het Nieuws van  
 den Dag voor

Ned. Indië. Batavia, Kalibesar.

Het Bataviaasch Nieuwsblad. Wel-  
 tevreden, Pasar Baroe Zuid.

De Java bode. Batavia, Stadhuis-  
 plein.

De Courant. Weltevreden, Gang  
 Pasar Baroe.

Weekly.

D'Orient. Weltevreden, Petjenon-  
 gan 72.



## *Under the Management*

of the

Netherlands India Hotel Company

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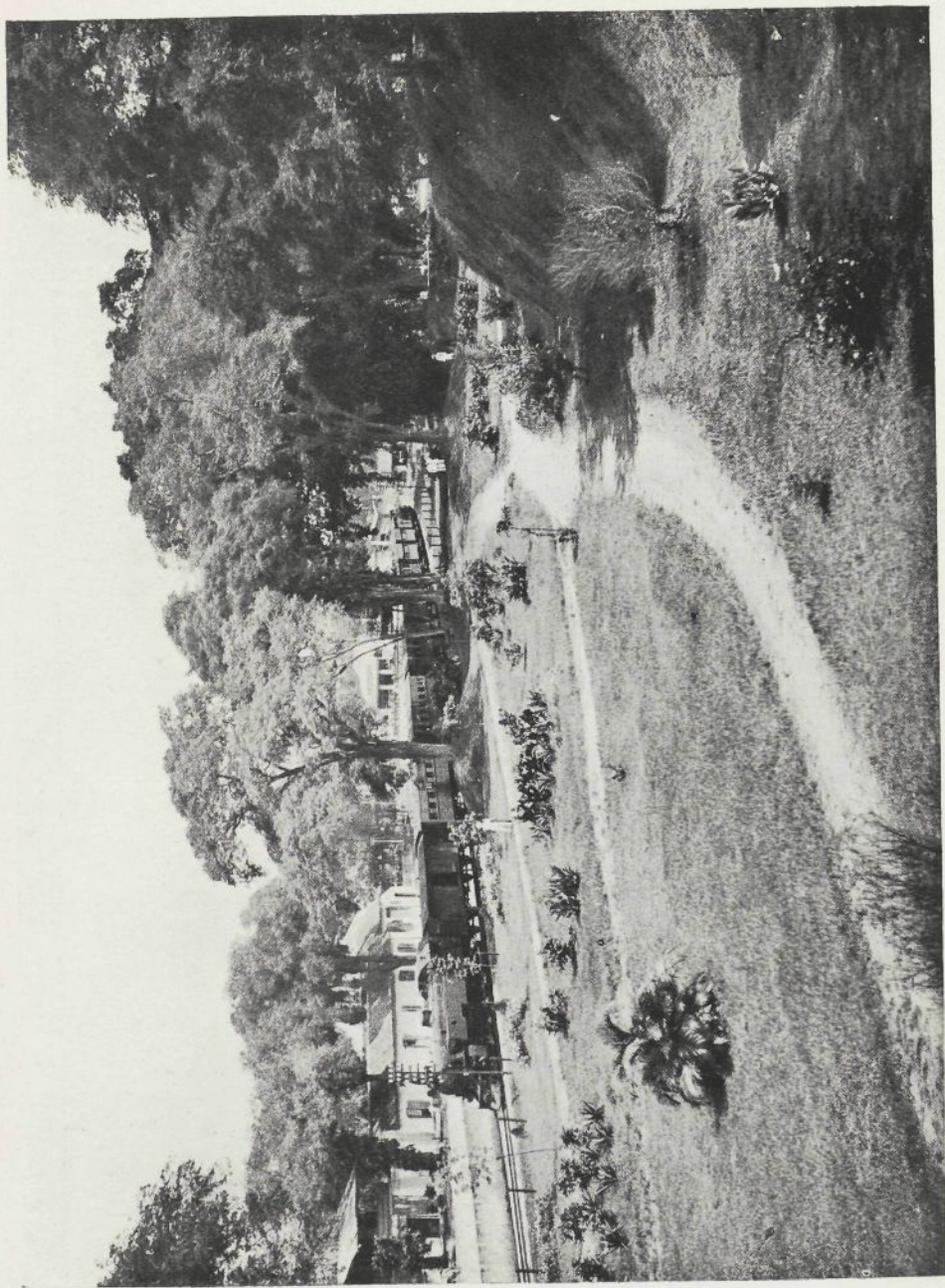
and

EXCELLENT SERVICE

### *RATES MODERATE*

(Glds 10.— and 11.—)

Porters meet all trains and steamers.



MODERN BATAVIA: VIEW ACROSS A CORNER OF THE WILHELMINA PARK IN WELTEVEDEN.

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Sport in Beeld. Weltevreden, Molenvliet.

*Monthly.*

Inter-Ocean (English). Weltevreden, Petjenongan 72.

Deutsche Wacht (German). Weltevreden, Noordwijk.

### Hotels & Restaurants.

Hotel Koningsplein, Koningsplein.

Hotel des Indes, Molenvliet.

Hotel der Nederlanden, Rijswijk.

Grand Hotel Java, Rijswijk.

Hotel Daendels, Koningsplein.

" Astor, Goenoeng Sari.

" Victoria, Tanah Abang.

" du Paviljon, Rijswijkstraat.

" Wilhelmina, Kebon Siri.

" Villa Park, Gang Scott.

Restaurant Versteegh & Rijkers, Noordwijk.

Oost Java Restaurant, Koningsplein.

Maison Cousel, Noordwijk.

(There are a number of small boarding houses in addition).

### Clubs.

Harmonie Club.

Concordia Club (Army & Navy).

Amicitia (Meester Cornelis).

Kali Besar Club (Batavia).

Batavia Swimming Club, Tjikini.

Bataviasche Sport Club, Koningsplein Noord.

British Club.

Batavia Cricket Club (The Box), Pegangsaan.

German Club (Deutsche Turnverein), Gang Thibault.

### Banking & Exchange.

Javasche Bank.

(The Java Bank), Batavia.

Ned. Handel Maatschappij, Batavia.

(Neth. Trading Society) branch office, Weltevreden.

Ned. Indische Handelsbank, Batavia.

(Neth. Ind. Commercial Bank) branch office, Weltevreden.

Ned. Ind. Escompto Mij. Batavia.

(Netherlands India Discount Bank). Branch office, Weltevreden.

Hongkong & Shanghai Banking Corporation, Batavia.

Chartered Bank of India, Australia & China, Batavia.

International Banking Corporation, Batavia.

Mercantile Bank of India, Batavia.

Yokohama Speciebank, Batavia.

Bank of Taiwan, Batavia.

Batavia Bank, Batavia.

### Shipping Comp. and Agencies.

K(oninklijke) P(aketvaart) M(aatschappij).

(Royal Packet Navigation Coy).

Head Office, Weltevreden.

Passage office, Weltevreden.

Branch office Batavia.

Branch office, Tandjong Priok.

Stoomvaart Maatschappij "Nederland".

(Nederland Royal Mail Line).

Head Office, Batavia.

Passage office, Weltevreden.

Branch Office, Tandjong Priok.

Internationale Crediet & Handelsvereniging "Rotterdam".

(International Credit & Trading Company "Rotterdam").

Agents for: the "Rotterdam Lloyd" Royal Mail Line.

the "Holland-American".

Passage Office, Weltevreden.

Branch Office, Tandjong Priok.

Java China Japan Line.

Office of Agents, Batavia.

Branch office, Tandjong Priok.

MacLaine Watson & Co. Batavia.

Agents for:

British India Steam.

Navigation Coy.

Blue Funnel Line.

British India and Apcar Line.

West Australian Joint Service.

Netherland Steamship Co. "Ocean".

Canadian Pacific Railway Ocean Services.

Asiatic Steam Navigation Coy.

Branch Office, Tandjong Priok.

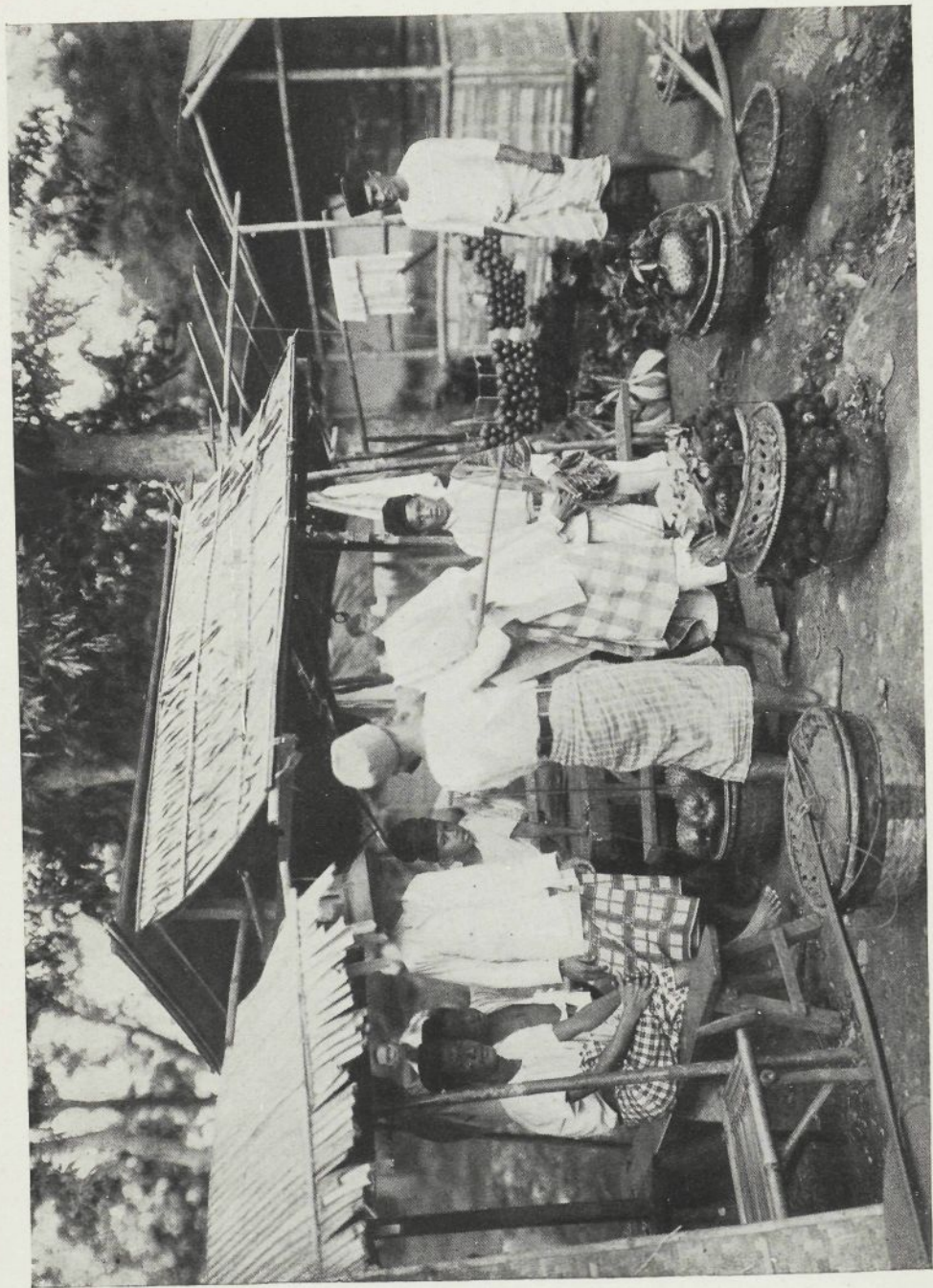
Ross Taylor & Co. Batavia.

United American lines.

Dollar Steamship Lines.

Prince Line.

Admiral Oriental Line.



NATIVE LIFE IN 20th CENTURY BATAVIA: THE FRUIT VENDORS.

# EIGEN HULP



WINKEL MAATSCHAPPY  
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Nippon Yushen Kaisha.  
American Pioneer Line.

Klaasen & Co. Batavia  
Agents for :  
Osaka Shosen Kaisha.

Borneo Company Ltd., Batavia  
Agents for :  
Toyo Kisen Kaisha.

Pitcairn Syme & Co. Batavia  
Agents for :  
Clan Line  
Ellerman and Bucknall Line  
R. P. Houston & Co.  
Bank Line  
Austral East Indies Line.  
West Austr. State Shipping Service  
Ismithian Line.

T. L. T. Platon, Batavia  
Agents for :  
Messageries Maritimes

Wilhelm Müller & Co., Batavia  
Agents for :  
Lloyd Triestino  
Deutsch Austr. D. Co.  
Kerr line

Behn Meyer, Batavia  
Agents for :  
Nord Deutsche Lloyd  
Stinnes Lines

Campbell Mac Coll & Co., Batavia  
Agents for :  
Burns Philp Line

#### Government Offices.

Department of Interior, Rijswijk.  
" " Finance, Waterloo  
Square.  
" " Justice, Rijswijk.  
" " of Public Works,  
Molenvliet

Naval Department, Goenoeng Sari.  
Department of Education, Water-  
loo Square.

Office of State Railways, Molen-  
vliet.

Court of Justice, Stadhuisplein,  
Batavia.

Highcourt of Justice, Waterloo  
Square, Weltevreden.

#### Public Buildings.

Museum and Library, Welt.  
Municipal Building, Weltevreden.  
Townhall, Batavia.  
Building People's Council (Volks-  
raad).

#### Consulates.

England. Kali Besar West, Batavia.  
Austria, Gumprich en Strausz,  
Prinsenstraat 16, Batavia.  
United States of America. Tanah  
Abang East 75, Weltevreden.  
Belgium. Malang Straat 19, Welt.  
China. Tanah Abang Heuvel 99,  
Weltevreden.  
Denmark. Koningsplein West 9,  
Weltevreden.  
France, (in charge British Consul).  
Germany. Koningsplein West 7.  
Weltevreden.  
Japan, Gang Holle 13, Weltevreden.  
Norway, Stoomv. My. Nederland,  
Kali Besar East, Batavia.  
Italy, Elisabeth Park 2, Welt.  
Siam, Borneo Coy, Ltd., Kampong  
Malaka 9, Batavia.  
Sweden, Tanah Abang Heuvel 140  
Weltevreden.  
Switzerland, Kampong Malaka 3,  
Batavia.  
Portugal, Ver. Prauwenveeren, Fac-  
torystraat, K P.M. building, Bat.  
Czecho Slovakia, Geo Wehry & Co.  
Batavia.

#### Recreations.

Theatre.  
Deca Park.  
(There are also a number  
of Cinemas).

#### Immigration Offices.

Office of Secretary, Batavia, Stad-  
huisplein.  
Office of superintendent, Depart-  
ment of Justice, Weltevreden.

AN UNSOLICITED TESTIMONIAL.

The following letter dated Oct. 25th 1925 from Mr. Irving E. Smith was received by the Burgomaster of Batavia :

Again as in several former years it has been my pleasure to spend some months in the Netherlands Indies, a part of the world that has ever held great charm for me. During my present visit I have spent most of my time in Weltevreden=Batavia. I love the Old Batavia for its many historic associations covering more than three centuries and have traveled its ancient thoroughfares quite thoroughly searching for historic landmarks sites of stirring events of the long ago when Batavia was one of the very few cities in this part of the world dominated by European influence and consequently a haven of refuge for overseas ships to put in to for repairs and supplies. The Museum in Weltevreden contains much that illumines Batavia as it was in the seventeenth and eighteenth centuries and I spent many hours in that Museum studying the relics on exhibit there, mute evidence of the activities of a people now long gone.

Weltevreden is a city of to day quite modern in all respect and the stranger within its gates senses the comforting assurance that he is in a city where law and order reign supreme and that no harm shall befall him while he may tarry here. Weltevreden's hotels, clubs, restaurants and places of amusement are excellent and quite up to date in the matter of service and accommodation.

Koningsplein is a splendid piece of open air space in the midst of the great city and to stroll around that immense square is sufficient exercise for one evening.

A wonderfully restful influence pervades the residential portion of Weltevreden with its well spaced dwellings and broad avenues lined by great shade trees whose high flung branches form a protective arch thereby making adequate defence against the rays of a tropic sun.

I like Java and her cities but mostly I care for Weltevreden perhaps because I know it best having been a visitor at various times covering many years.

On several occasions I have been in Weltevreden when the annual Pasar Gambir fair was in progress. Sometime I should like to tell you something about what the foreign visitor observes that the citizen does not observe as it is commonplace to him.

You have a fine city and I enjoy a visit within its hospitable walls and trust it may be my privilege and pleasure to return there again another year.

My best wishes for the prosperity and welfare of your beautiful city.

Sincerely  
IRVING E. SMITH.

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# IMPORTANT ADVICE FOR TOURISTS

When a steamer arrives at Tandjong Priok (Java) it is swarmed by people anxious to make arrangements for tourists. It should be noted by those concerned that they can save

# 50%

of their money when they make their own arrangements and hire a car from

THE JAVA  
AUTOMOBILE SERVICE  
WELTEVREDEN

42 Kebon Sirih — Phone Wl. 3651

Cable-address: JAS.

# *Java Government Railways*

Every tourist, who stays for a longer or shorter period in Batavia, should make a trip to Bandoeng and Garoet by

*Java Government Railways!*



Comfortable trains with dining cars run on this route, famous for its beautiful views, twice a day. It is

The  
*Scenic Route of West Java.*

*Both in Bandoeng and Garoet*

excellent hotel accomodation,  
charming environs, bracing climate.